

**WARD:** Easton **CONTACT OFFICER:** Paul Haworth  
**SITE ADDRESS:** Social Services Site (Formerly Avonvale Primary School) Avonvale Road Bristol BS5 9RH

**APPLICATION NO:** 14/00649/F Full Planning  
**EXPIRY DATE:** 16 May 2014

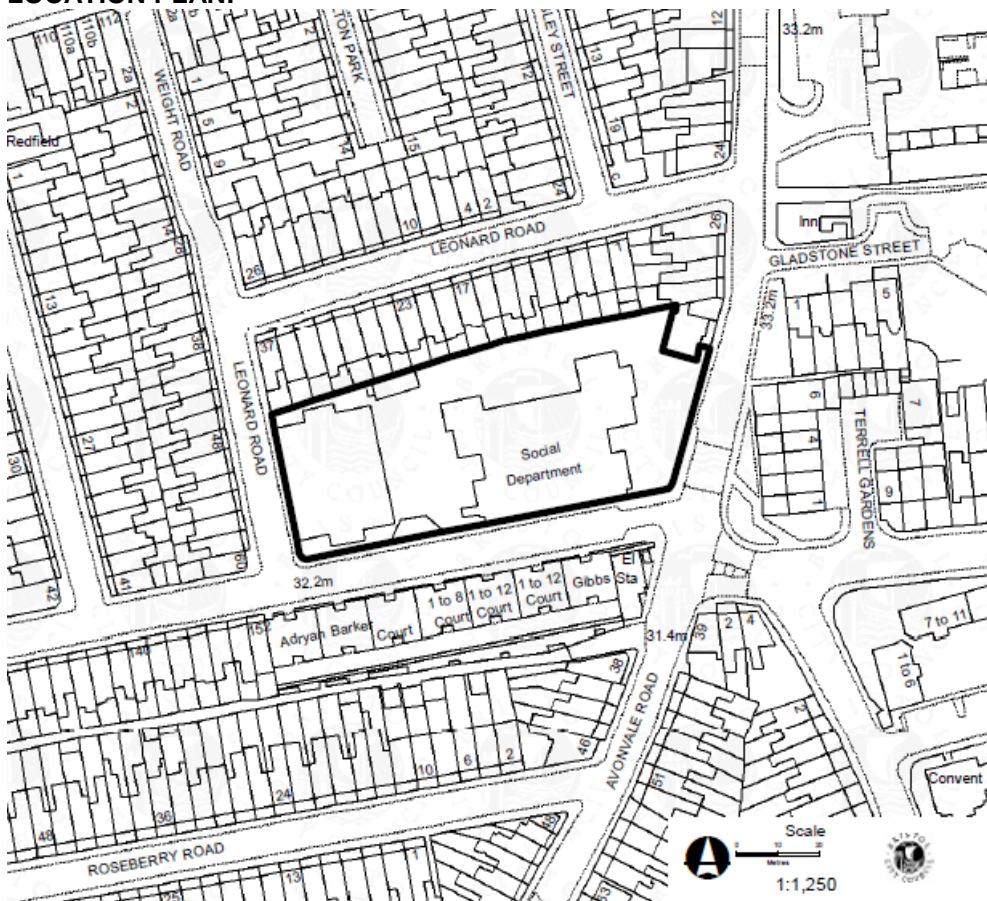
**Demolition of existing buildings, retention of western building for one year as a temporary home for the reception classes of the new school and subsequent demolition. Erection of new 2FE Primary school and associated external works. New service access to Avonvale Road and associated highway improvements.**

**RECOMMENDATION:** Grant subject to Condition(s)

**AGENT:** Batterham Matthews Design  
1 Tollbridge Studios  
Tollbridge Road  
Bath  
BA1 7DE  
**APPLICANT:** Bristol City Council  
BSF Team CYPS  
City Hall  
College Green  
Bristol  
BS1 5TR

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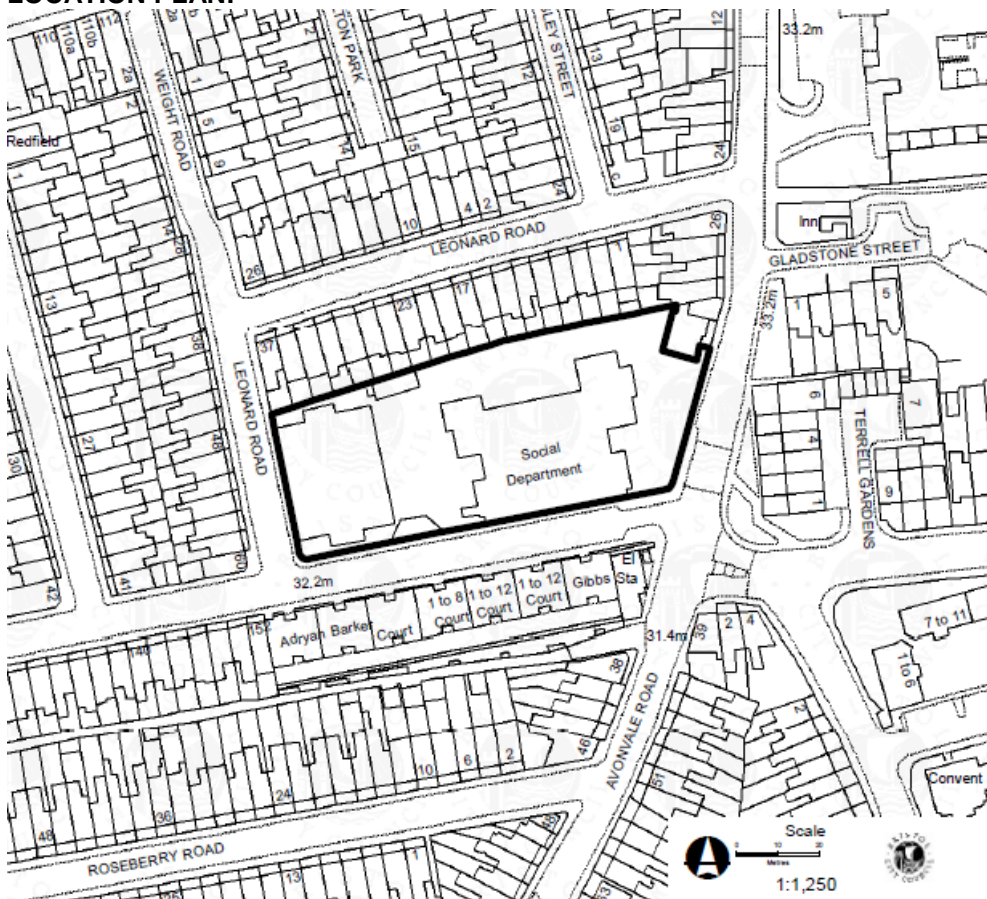
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LOCATION PLAN:



**Development Control Committee A – 18 June 2014****Application No. 14/00649/F: Social Services Site (Formerly Avonvale Primary School) Avonvale Road Bristol BS5 9RH****SUMMARY**

This is a full, detailed, planning application for the re-development of the existing BCC Social Services (offices) Department. The proposal involves the demolition of all the existing Victorian former school buildings and the re-development to create a new 2FE primary school. The new buildings would range from 2 to 3 stories and border Victoria Avenue and Avonvale Road as a perimeter block.

The educational need for a new primary school in this location is high, being located between the Ashley, Easton and Lawrence Hill Neighbourhood Partnership (NP) Area and the St George East and West Neighbourhood Partnership Area. These NP areas have seen a significant rise in school age children, with a projected shortfall by 2017 being equivalent to 9 forms of entry (270 children each year, if no action is taken). The proposed school would contribute significantly to meeting the shortfall with provision for 2 forms of entry (60 children each year).

The existing buildings contribute greatly to the character and appearance of the surrounding area and can be classified as 'non-designated heritage assets'. Their demolition is regretful, but the buildings are not protected from demolition as they are not listed or within a Conservation Area. The initial objection to the design and appearance of the building has been resolved as a result of an amended scheme. There are a number of objections raised from local residents that mainly relate to the principle of demolition, the design of the proposed buildings and highway safety.

All of the objections have been carefully considered and the application is recommended for approval, subject to conditions and an agreement to secure highway works in the vicinity of the site.

**SITE LOCATION AND DESCRIPTION**

The (0.41 hectare) site is located approximately 2.5km east of the city centre, on the corner of Victoria Street and Avonvale Road, within the Easton Ward of the city. The site is not within a Conservation Area and no buildings on or adjoining the site are listed. The school was built in 1898-9. The architect was Herbert J Jones, who built many of Bristol's Methodist chapels from the 1880's. Four buildings exist on site; two main former school buildings, a two-story former dwelling that faces onto Avonvale Road and a small workshop/store building located against the northern boundary of the site. All of the existing buildings are stone built buildings that contribute to the character of the surrounding area, and have been nominated for inclusion on our local list.

The predominant uses surrounding and bordering this site are residential dwellings. Victorian terraced dwellings along Leonard Road adjoin the northern boundary of the site, located to the south of the site on the opposite side of Victoria Avenue is a collection of three and four story flats. Located to the east of the site, on the opposite side of Avonvale Road are two story terraced dwellings of Terrell Gardens and Gladstone Street. St Patricks Primary School lies to the south east of the site off Pilemarsh, although there are no other non-faith primary schools within the immediate vicinity of the site.

The main shopping and commercial area of Church Road lies within easy walking distance, approximately 1,000 metres to the north of the site. The site lies within a 'High Risk' coal mining area and within Flood Zone 1. A copy of the 1:1250 scale site plan showing the footprint of the existing school buildings and the immediate context is attached as Supporting Document 1.

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## THE APPLICATION

Full planning permission is sought to demolish the existing Victorian school buildings and re-develop the site with a new 2FE primary school.

The development would be completed in 2 phases, with the retention of the existing western building for 1 year as a temporary home for the reception classes of the new school, whilst the remaining buildings on site are demolished and the construction of a new school begun. Phase 2 will commence after completion of the new building, which involves demolishing the western building and then constructing the playground.

The new building would range from 3 stories in height along Avonvale Road and two stories along Victoria Avenue.

The proposed external materials comprise of a combination of natural stone, timber (cedar) cladding, glass, and rendered walls. All of the mono-pitched roofs would be covered with a standing seam covering, with all flat roofs being covered with a single ply membrane. All new windows and doors would be powder coated aluminium.

A sprinkler tank would be located in the north east corner of the site, close to the end gable elevation of 32 Avonvale Road. The refuse storage area would be located alongside (to the west of) the sprinkler tank in an enclosed area. To the north of the sprinkler tank and refuse storage area would be a secure area of staff cycle parking (6 cycle spaces) facility. There would be a pedestrian access from Avonvale Road between the sprinkler tank/refuse storage area and the kitchen, for use by staff and deliveries.

The main visitor entrance would be positioned on the Avonvale Road elevation, opposite the zebra crossing. The existing vehicular access onto Victoria Avenue at the western end of the site would be retained, but reduced in width to become the main pupil entrance.

The footprint of the proposed building would be set back from the existing (retained) site boundary walls into the site by 1.2m along Victoria Avenue and 1.6m along Avonvale Road. This would result in a 3.0m wide pavement along Victoria Avenue and a 2m wide pavement along Avonvale Road, with the existing wide pavement splay at the corner of these two roads maintained. Landscaping would be provided between the retained boundary walls and the proposed school along both Avonvale Road and Victoria Avenue.

Within the site would be a newly formed playground, with a surface marked games court located at the western end of the site. Covered cycle and scooter parking facilities would also be located within the site, set against the southern boundary in the far south western corner of the site. Located in the North West corner of the site would be a 210m<sup>2</sup> habitat area that would be developed by the school. The habitat area would incorporate a hedge along the boundary of properties (38-44 and 73 Leonard Road) that border the north of the site.

Six trees are proposed to be planted within the playground close to the northern boundary of the site, together with a hedge that would be sited along a 22m section of this boundary.

The existing western former school building currently forms the boundary along part of Leonard Road. Following the demolition of this building, the existing stone that forms the end wall of this building would be retained up to cill level of the existing window openings (which would be in-filled with railings).

A series of solar panels (facing south) would be positioned on the flat roofed section of the building fronting onto Victoria Avenue. The main 'feature' of the proposed building is a natural stone 'fin' wall

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that would be located near the main visitor entrance at the corner of Victoria Avenue and Avonvale Road. This replaces the 3-storey cylindrical 'tower' that was part of the original design.

The visitor entrance would be positioned between the stone fin wall and the hall/teaching block located to the north along Avonvale Road. The main visitor entrance would have curtain glazing from ground through to second floor level, with canopies above the ground floor entrance and at the top of the second floor level.

The school hall would be a double height space located in the building that is situated along Avonvale Road. The classrooms for reception and years 1-5 would be located at ground floor level in the section of building located along Victoria Avenue, with year 6 classrooms located at second floor level above the hall in the section of building that is located along Avonvale Road.

A selection of the proposed plans is included as Supporting Document 2.

Alongside the plans and drawings, this application was accompanied with the following supporting documents:

- Community involvement statement
- CIL questionnaire
- Air quality report
- Arboricultural impact assessment
- Archaeological desk-based assessment
- Construction management statement
- Coal Mining risk assessment
- Breeam report
- Design and access statement
- Drainage Statement
- Economic statement
- Energy sustainability strategy statement
- Ecology report
- Landscape strategy
- Site waste management plan
- Transport assessment
- Travel plan (interim)
- Ventilation statement
- 3D visuals

#### RELEVANT PLANNING HISTORY

13/02139/PREAPP: Pre-application enquiry that looked at 3 different options for the re-development of the site with a 2FE primary school. The discussions included conversion schemes that retain all or parts of the existing buildings and reasons put forward why these were not viable options.

#### COMMUNITY INVOLVEMENT

##### A) Process

As part of the pre-application process, the applicant and agent undertook community consultation, which included consultation with 'Planning Solutions'. The submitted Community Involvement Statement provided by the Chair of Planning Solutions states the following;

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"Planning Solutions has been involved in discussions over this development since it was first proposed in 2013, representatives from Skanska and their associates have attended two of our group meetings, and we have also taken part in their public consultation sessions and been present when they presented their proposals to the Neighbourhood Forum.

Whilst initially we felt that some aspects of the existing old school buildings might be worth saving or incorporating, we were also aware that the need for extra school places in the area is paramount, and we are pleased that the proposed design will make use of stonework and some of the existing walling.

We have consulted several local groups and the Barton Hill Society, and the general opinion is that whilst some local people can remember going to school there, they mainly feel that the provision of a new school on this site is paramount.

Our group has, therefore, no hesitation in supporting this development".

**B) Outcomes**

The meetings and engagement that has taken place with the Local Neighbourhood Partnership and residents has clearly highlighted the two principle issues of the value of the existing school buildings versus the creation of a new modern school to accommodate educational need in this part of the city. During the pre-application enquiry, the agent indicated that Local parents were very keen to get a new school up and running on this site. Less objection than has currently been expressed under the formal planning application was reported.

In your officer's opinion, the community engagement has not necessarily shaped or influenced the final scheme before members today.

**RESPONSE TO PUBLICITY AND CONSULTATION**

In respect of our formal consultation, the following can be confirmed.

Two site notices were displayed (1 located on Avonvale Road and 1 on Victoria Avenue) on 5 March 2014, with an expiry for receipt of comments of 26 March 2014. A press advertisement was also undertaken with the same period for comment.

304 letters were sent to local residents and businesses that surround this site, with an expiry date for receipt of comment of 25 March 2014.

**REVISED PLANS:**

The applicant amended their proposal and submitted revised plans in response to objections relating to the design of the submitted scheme. This amended scheme has been subject to re-consultation that was undertaken on 12 May 2014, with an expiry for receipt of comments of 2 June 2014.

The changes can be summarised as follows:

- \* Change to external design and appearance of the building, including the removal of the circular stair case near the main entrance.
- \* Change to the external materials.
- \* Retention of the existing stone boundary walls along Avonvale Road frontage (and part of Victoria Avenue frontage), including the piers and entrance point.
- \* The inclusion of landscaping behind the retained boundary walls.
- \* Re-siting of the cycle parking stands within the site, behind the boundary wall.

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\* Alteration to the siting of the sprinkler tank and service yard area at the north east end of the site. The inclusion of a pedestrian access route between the service yard and sprinkler tank and the adjoining property at 32 Avonvale Road (removal of a vehicular access point).

\* Repositioning of the solar panels along the building proposed along Victoria Avenue.

[The changes made have not altered the scale or massing of the proposed building or its footprint].

In response to both consultations undertaken, and at the time of writing this report a total of 25 representations have been received. These comprise of the following: 14 objections, 8 in support, and 5 neutral responses. Out of the 14 objections recorded, 11 of these were submitted following the amended scheme being consulted on. Of these 12 objections, comments still related to the poor design, the fact that the existing Victorian school buildings should be retained and converted and concerns on highway safety grounds.

The comments received can be summarised as follows:

**SUPPORT:**

**Principle/Use:**

This new school is vital for the families in this area of the city and will strengthen the sense of community.

**Layout and design:**

The layout makes best use of this site, ensuring that the playground is shielded from the two busy roads (Avonvale Road/Victoria Avenue) that bound this site.

The building has an attractive frontage and makes best use of (re-claimed) stone and timber.

The building would be a monolithic structure that impacts upon the amenity of flats on the opposite side of Victoria Avenue.

**Landscaping/trees:**

Support for new tree planting on-site (Leonard Road resident).

Good to see 10 new trees being planted (none being felled). Ensure tree pits allow the trees to grow to maturity, rather than underground containers.

**OBJECTIONS:**

**Demolition/Layout/design:**

Demolition and new-build is not a sustainable form of development. It would be a waste of precious and finite energy resources, when the existing school could be retained and re-used.

The existing buildings are a local asset and should be protected from demolition.

The loss of the existing buildings contravenes local policy and advice and guidance from English Heritage.

Loss of existing characterful buildings: It would be better to retain and retro-fit/convert the existing buildings.

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There are many examples of Victorian buildings that have been adapted for school use.

This needs to be a landmark building to justify the loss of the existing Victorian school buildings.

The main entrance is unattractive and institutional in its appearance; not in-keeping with the primary school.

The 'drum' near the main visitor entrance has potential to get dirty very quickly (drum now removed from the scheme).

Frontages of building are not welcoming, stark and need improvement (now amended).

The proposal is a brutal looking building that needs to be more creative in its appearance (now amended).

Poor quality outdoor space, bland unimaginative use of the play area. Greater use of roofs and walls should be looked at.

The proposed habitat area too small to be of any value.

Could more elements/features of the existing buildings be saved and incorporated into the new building?

The proposal is contrary to the NPPF and development plan policies designed to protect buildings of local character and heritage assets.

Highways:

Existing road layout around the school needs careful consideration to minimise dangers for pedestrians.

Concerns over the parking situation, given that nearby streets are already heavily congested with on-street parking. Where will parents and staff park?

Avonvale Road and Victoria Avenue are used as 'cut through' routes and traffic management measures need to be considered [to safeguard children].

The junction of Victoria Avenue and Avonvale Road is an accident waiting to happen. It's too narrow; cars mount the pavement turning into Victoria Avenue.

An improved crossing on Avonvale Road is required, particularly given the volume of traffic that uses Avonvale Road and that it is used as a 'cut through' from Pilemarsh to St Phillips.

Inconsiderate parking already occurs from parent's dropping-off/collecting children from St Patricks School; this proposal will worsen this situation in this area.

School use:

420 school places at such a small site seems to be an over intensive use and the proposals should be scaled back.



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## Landscaping/trees:

The trees within the site, along the boundary of the properties of Leonard Road may block light into gardens/houses, the roots could undermine the stability of the boundary wall and branches could overhang the boundary. The current wall is high enough to protect privacy and low enough to allow sufficient light into the rear gardens.

It is a pity that the roofs are not being utilised as roof gardens or green roofs.

## E-petition:

In addition to the Council's consultation process, an on-line e-petition via change.org has been set-up against the demolition of the existing buildings. The petition is entitled; 'Don't demolish Avonvale Road Board School'. At the time of writing this report, this e-petition has attracted 126 supporters. Members of the Committee have been made aware of this e-petition and provided with the necessary link to enable them to read the comments and see the number of comments posted.

## Listing:

Members should note that during the course of the assessment of this application, an application for listing was made to English Heritage. This has been considered and the decision made is not to list these buildings. Further information on this issue is provided below.

## OTHER COMMENTS

**Transport Development Management, City Transport** has commented as follows:-

No objections based on highway safety improvement works being secured and conditioned.

The highway safety improvement works, include the provision of a raised zebra crossing on Avonvale Road (where there is currently a traffic island), widening of the footways around the site, changing surfacing treatments to the Victoria Avenue junction, and new school keep clear road markings, and marked disabled and coach bay parking points on Victoria Avenue.

Planning conditions are required to secure the submission of a detailed school travel plan within 6 months of occupation of the new school, and secure the provision of the cycle and scooter parking facilities.

No objections raised to the location of the servicing access/area.

04.06.14: Comments received in relation to revised plans.

No objection to the revised proposals. This will remove the need to move the lighting columns and utility boxes. The revised highway works plan should be conditioned and attached to the new revised Unilateral Undertaking.

**Urban Design** has commented as follows:-

It is regrettable that the scheme requires the total demolition of the existing school buildings, which are considered to be of some architectural interest and attractive features within the local townscape. However the CDG recognise that the needs of a modern primary school in terms of internal teaching and support spaces precludes their remodelling for on-going use. Therefore, although the CDG do not support the removal of these buildings in terms of their contribution to local character and identity we are not opposed to their replacement with a new school.

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With reference to the new school, earlier iterations of the scheme had presented several concerns in terms of the design, specifically the arrangement of the buildings elevations and massing when seen from the surrounding streets. The impression given by earlier versions was of an overly introspective and fortified building, not presenting the open welcome that would be expected from a primary school.

The CDG are pleased to see that the designer has taken on board the principal design concerns raised and has responded by making some significant changes to the elevational arrangement, massing and fenestration of the scheme that have led to a more attractive, active and welcoming building. The simplification of the materials palette to stone, timber cladding and render is an improvement and the omission of the panellised cladding system is a positive change.

Overall the scheme presents a more ordered and outward looking result. At this stage, CDG support the current application scheme in terms of its layout, elevational arrangement and appearance.

It is recommended that the architectural detailing and materials are conditioned.

**Pollution Control** has commented as follows:-

If minded to approve, conditions are required on the following matters:

1. Construction Management Plan
2. Noise from plant and equipment
3. Sound insulation
4. Ventilation

**Sustainable Cities Team (Mark Leach)** has commented as follows:-

No objection. Policy compliant, but with some areas for improvement.

#### Renewable energy

A comprehensive Energy Strategy has been provided. The applicant proposes to meet 20% of the residual energy through the provision of solar PV on the south facing roof. The applicant has considered orientation, inclination and shading (PV panels to be conditioned).

The lighting would have automatic daylight dimming controls and presence/absence detectors to avoid lighting unused areas, which in combination with energy efficient bulbs, should see an additional saving of 14% from the lighting load. A renewable energy display in the school reception area would be provided, as required within Bristol City Council's Sustainable School Specification.

The development would achieve BREEAM 'Very Good'. The applicant is encouraged to achieve BREEAM 'Excellent' (the agreed BREEAM standard to conditioned).

#### Waste & recycling

The site waste management plan outlines targets to recycle/recover 97% of demolition waste. I note the re-use of recovered stone from the existing building to feature in the new design - greater use of the existing building stone to feature in the design of the new build rather than timber cladding would be welcomed.

#### Water

The applicant proposes to reduce water consumption by 25% through the use of dual flush WC's, spray, aerator and time controlled taps and flow restrictors. Water consumption would be reduced by 25% which is commended.

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## Biodiversity

The habitat area is welcomed. Softer (greener) sustainable drainage options within the landscaping, such as a green roof, living wall, swales, attenuation ponds are encouraged.

## Information &amp; communications technology

The use of smart metering is welcomed.

28.05.2014 - Comments on revised plans:

Welcome revision to retain more of existing stonework and reuse more stone on site. The additional planting will go some way to improve green infrastructure, biodiversity and reduce surface water run-off. Disappointed to see previous concerns over BREEAM Excellent or rainwater harvesting have not been able to be included.

**Archaeology Team** has commented as follows:-

The former school lies in an area of possible archaeological significance. A number of late Roman burials were located to the south of the site, initially in the late 19th century and then more recently in 2002 and 2007. These were all found on the south side of Victoria Avenue. The presence of these burials, especially those found in the 19th century, which were found in rows, strongly suggests the presence of a Roman cemetery of unknown dimensions. There were further indications of Roman settlement from a limited evaluation to the east of this site. This work, especially the occurrence of burials close to the site, suggests that similar finds might be made here.

An archaeological evaluation must be carried out as early as possible to define the archaeological potential of the site and allow for an informed decision on the most appropriate form of mitigation to be adopted.

The school building itself has been nominated for the Bristol Local List and is clearly a building of some architectural merit. Its loss would run counter to the spirit of draft development management policy DM31, which seeks the preservation of heritage assets including locally listed heritage assets. This accords with NPPF paragraph 135, which seeks a 'balanced judgement', in the consideration of harm to such an asset, and with Core Strategy policy BCS22.

If the building cannot be saved, it is important that it is fully recorded both internally and externally to provide a permanent record, although such a record cannot be a substitute for the building itself. Such works should be secured by appropriate conditions.

**Crime Reduction Unit** has commented as follows:-

Comments made in relation to original scheme: Objection to the retention of utility boxes in middle of newly created (wider) footway, with benches placed either side of these boxes. This could create a gathering point which could lead to anti-social behaviour, places to loiter near a school entrance.

Urge developer to apply for secured by design certification as this will ensure minimum standards of security.

02.06.14 - Update on amended scheme - Addresses previous objections, a much better scheme.

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**Flood Risk Manager** has commented as follows:-

No objections, subject to the submitted drainage strategy in relation to discharge rates being updated and a condition that requires a detailed drainage design scheme to be approved by BCC prior to the commencement of development.

**English Heritage** has commented as follows:-

[No consultation was necessary with EH, but an application for listing the existing buildings was made to English Heritage during the assessment of this application].

Decision: The Secretary of State has decided not to add Avonvale Road Board School to the List. The following principal reasons are given:

Date: Large numbers of board schools were built from 1870 and many survive nationally. This example of 1898/9 is a late example of a relatively common building type.

Architectural interest: Whilst having some individual features of merit, the design overall is not particularly accomplished or innovative in the national context.

Interior fittings: Whilst the extent to which original fittings survive is not known, they are likely to have been replaced as needs have changed over the buildings life. Even if the buildings interior were to survive complete, this would be unlikely to raise enough to meet the strict criteria for listing.

Historic interest and association: Thomas Macnamara has no links with the buildings, nor do any other figures of national note. The historic interest lies on a local rather than a national level.

**The Victorian Society** has commented as follows:-

Object to the application on the basis that it would result in the loss of a complex of buildings with heritage value to Bristol. Although the buildings are not listed, they clearly contribute historically and architecturally to the area, and should be considered a heritage asset.

The NPPF section 136 states that the effect of an application on a non-designated heritage asset must be taken into account in deciding planning applications. Section 131 states that "local planning authorities should take into account the desirability of sustaining and enhancing heritage assets" and "the positive contribution that conservation of heritage assets can make to sustainable communities".

We appreciate that the provision of a new school is a significant public benefit. The best outcome would be for the local community to benefit from both the new school and the retention of the existing building. It has not been shown that the loss of this undesigned heritage asset is required to achieve the benefit, and that the school could not be accommodated in the existing buildings, or on a different site.

We therefore recommend that the application is refused. A solution should be sought which retains some or all of the buildings on site.

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## RELEVANT POLICIES

**National Planning Policy Framework – March 2012****Bristol Local Plan, Adopted December 1997**

B5	Layout and Form
B2	Local Context
B6	Building Exteriors and Elevations
B22	Sites of Archaeological Significance
EC4	Protection: Existing Employment Opportunities
M1	Transport Development Control Criteria

**Bristol Core Strategy (Adopted June 2011)**

BCS8	Delivering a Thriving Economy
BCS10	Transport and Access Improvements
BCS11	Infrastructure and Developer Contributions
BCS12	Community Facilities
BCS13	Climate Change
BCS14	Sustainable Energy
BCS15	Sustainable Design and Construction
BCS16	Flood Risk and Water Management
BCS21	Quality Urban Design
BCS23	Pollution
BCS22	Conservation and the Historic Environment

**Bristol Site Allocations and Development Management Policies (emerging)**

DM1	Presumption in favour of sustainable development
DM5	Protection of community facilities
DM14	The health impacts of development
DM23	Transport development management
DM26	Local character and distinctiveness
DM27	Layout and form
DM29	Design of new buildings
DM32	Recycling and refuse provision in new development
DM33	Pollution control, air quality and water quality
DM34	Contaminated land

## KEY ISSUES:

- A) IS THE LOSS OF THE EXISTING OFFICE USE ACCEPTABLE AND WOULD THE SCHEME MEET THE EDUCATIONAL NEEDS OF THIS LOCALITY?

Loss of existing office use:

The existing buildings were originally built as a primary school and remained in this use for approximately 75 years, although the buildings were last in use as offices for the Council's Social Services department. Given the authorised use is as offices, the removal of this use has to be considered against policies EC4 and BCS8. Policy EC4 permits the loss of employment floorspace, subject to one of a five point criteria being met and BCS8 seeks to retain employment floorspace where it provides a valuable contribution to the economy of the area.

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The agent provided an Economic Statement in support of the proposal. In summary, this statement indicates that;

- a. There is adequate availability of office space in Bristol at present and in the near future (The office staff that were in this building have already been moved to alternative accommodation in the city);
  - b. The existing offices would be difficult to lease as offices on an on-going basis due to incoming energy efficiency requirements;
  - c. The change of classification from office to a school is permissible under current planning guidance;
- and
- d. There is a shortfall in primary school places in the Redfield area of Bristol.

In this instance it is considered that the proposal meets the first criteria listed under EC4; which states that the floorspace to be lost is not valuable, in terms of quality or quantity, to the stock of employment floorspace in this area. In turn, this means that the change of use would not conflict with policy BCS7 of the Core Strategy that seeks to retain employment land, "where it makes a valuable contribution to the economy and employment opportunities".

Moreover, the recent changes made by Government in May last year to the Town and Country Planning (General Permitted Development) (Amendment) (England) Order 2013, means that the permanent change of use of existing B1 premises to a state-funded school is now classed as 'permitted development', subject to a Prior Approval process being undertaken and being confirmed as acceptable against highway and transport, and noise grounds. Whilst the applicant has not been through this Prior Approval process, given that this site was formerly a primary school, it is very likely that any Prior Approval application would be acceptable on highway and noise grounds, and this is a material consideration in the determination of this issue.

In conclusion, the loss of the authorised and last use as offices is considered to be acceptable and policy compliant. The previous (historical) and proposed use of this site also weigh in favour of this issue.

**Educational Need:**

The applicant has indicated that the demand for school places in this local area is so high that temporary provision has to be made for new Reception Classes for enrolment in September 2014 prior to the new school being constructed.

The applicant was asked to clarify, in respect of this part of the city, the educational need and how this site assists in meeting this need. In response, the applicant stated the following:

"The Local Authority has a statutory duty under the School Standards and Framework Act 1998 to ensure there are sufficient school places, diversity in provision of schools and increased opportunities for parental choice.

Bristol produces a School Organisation Strategy that sets out how the demand for school places will be met. For Primary School provision the Strategy is organised in Neighbourhood Partnership Areas and data is presented on this basis. It is recognised that the actual pattern of school attendance is more complex and many children will cross Neighbourhood boundaries to access school places.

Bristol has seen a rapid growth in demand for primary school places with a 30% rise in the number of pupils entering the Reception Year in the last 5 years. This equates to more than 1,000 extra pupils

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requiring school places in each year group. The majority of this growth has been accommodated by expansion of existing schools and academies but there is now a need to open new provision in some areas of the city.

The Avonvale Road site is located very close to the boundary between the Ashley, Easton and Lawrence Hill Neighbourhood Partnership Area and the St George East and West Neighbourhood Partnership Area. These areas have seen some of the most significant rises in numbers of children. Both areas are showing a significant shortfall of places if no further action is taken. The projected shortfall across both areas by 2017 is equivalent to 9 forms of entry (270 children each year).

The majority of schools within the Ashley, Easton and Lawrence Hill and St George areas have already been expanded or have sites that mean expansion could not be achieved cost effectively. Provision on an additional new site is therefore essential.

The new academy at Avonvale Road will contribute significantly to meeting the shortfall with provision for 2 forms of entry (60 children each year). The site is particularly well-placed being located on the boundary between the Neighbourhood Partnership areas".

The table attached as Supporting Document 3, shows the number of places available in the Reception year group and the forecast demand for places to 2017.

At the heart of the National Planning Policy Framework is a presumption in favour of sustainable development, which this guidance advocates as being the 'golden thread' running through both plan-making and decision-taking.

The NPPF states that the Government attaches great importance to ensuring that a sufficient choice of school places is available to meet the needs of existing and new communities, and that local planning authorities should take a proactive, positive and collaborative approach to meeting this requirement, and to development that will widen choice in education. Great weight should be given to the need to create, expand or alter schools.

It is clear from this information that there is a very real need to provide an additional primary school in this part of the city; and this proposal will undoubtedly help meet the need for school places in this area. This will enable local families to send their children to attend a local school, which helps build strong communities and curb longer travel distances undertaken by parents and carers to take children to schools outside of this neighbourhood area.

In conclusion, a new 2FE school is required in this part of the city to assist with meeting educational need. This school would, therefore, provide a sustainable form of development, which has been attributed significant weight in the determination of this application.

**B) IS THE DEMOLITION OF THE EXISTING BUILDINGS ACCEPTABLE?**

The buildings are not situated within a Conservation Area and they are not listed; because of this they have no protection against demolition. They are, however, clearly characterful buildings that contribute greatly to the character and distinctiveness of this area to an extent that makes the school site a local landmark.

As stated in the site location and description part of this report, the existing school buildings have been nominated to be placed on a local list of 'non-designated heritage assets'. This is a list that is being compiled in response to concerns about the loss of much-loved elements of the local landscape, such as pubs, chapels and industrial buildings. These help to reinforce the local identity of a place, and can often make them more attractive for investment and generally make them better places to live and work. A local list will help to highlight those places which local communities feel to

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be worthy of preserving when it comes to drawing up Neighbourhood Plans or when planning applications for specific sites are being considered.

Buildings that feature on a local list are assets that are not protected in law as Listed Buildings, Scheduled Ancient Monuments or through their location within Conservation Areas. Heritage assets are defined in the National Planning Policy Framework (2012) as;

"A building, monument, site, place, area or landscape identified as having a degree of significance meriting consideration in planning decisions, because of its heritage interest. Heritage asset includes designated heritage assets and assets identified by the local planning authority (including local listing)".

Buildings on a local list must be taken into account in determining a planning application (National Planning Policy Framework, paragraph 135) but clearly do not carry as much weight as a designated heritage asset. Nevertheless, they are important considerations in terms of the contribution they make to local distinctiveness and their potential importance to the local community.

Policy BCS21 of the Core Strategy states that development should contribute positively to an area's character and identity, creating or reinforcing local distinctiveness. Policy DM26 of the Site Allocations and Development Management Policies expands on this requirement, by setting an eight-point criterion against which a development's response to local character and distinctiveness will be assessed. This policy also states;

"Development will not be permitted where it would be harmful to local character and distinctiveness or where it would fail to take opportunities available to improve the character and quality of the area and the way it functions.

Development should retain existing buildings and structures that contribute positively to local character and distinctiveness. Where the loss of an existing building or structure is acceptable due to the wider planning benefits of a development proposal, opportunities should be sought to reuse any materials that contribute to local character and distinctiveness".

There is no doubt that this proposal would result in the loss of locally distinctive and characterful buildings that positively contribute to the identity of this site and the immediate surrounding area, and their demolition does not accord with parts of policy BCS21 or DM26. However, the buildings are not listed (and English Heritage have recently confirmed that the buildings are not suitable for listing), nor are they within a Conservation Area, and the wider planning benefits of this proposal are judged to outweigh the objection against parts of BCS21 and DM26. Notwithstanding this point, your officers did hold a series of meetings with the applicant during the course of the pre-application enquiry process that centered around exploring ways to retain all or some of the existing buildings and convert them for modern educational use. For various reasons, there was no retention or conversion scheme that was deemed a realistic option or way forward at this site to accommodate the size of school that is needed. In addition, and in accordance with the second part of DM26, the amended scheme does retain the existing stone boundary walls and the side of the building that forms the boundary to Leonard Road. Moreover, stone taken from the existing buildings would be re-used in the feature fin wall at the main entrance to the school. The retention of the boundary walls and the re-use of this material provide a degree of historical reference to the previous school and its materials.

For all of the reasons set out above, the demolition of the existing buildings is considered acceptable in order to contribute towards meeting the educational needs of the city.



**Development Control Committee A – 18 June 2014****Application No. 14/00649/F: Social Services Site (Formerly Avonvale Primary School) Avonvale Road Bristol BS5 9RH****C) IS THE DEVELOPMENT ACCEPTABLE ON URBAN DESIGN GROUNDS?**

This key issue assesses the design merits of the proposed school buildings, and not the demolition of the buildings that is dealt with under Key Issue B above.

On the basis that there is no way of retaining the existing buildings and adapting them into a new 2FE primary school, advice was provided during the pre-application stages that indicated any replacement school be at least of equal quality in terms of contribution to local character and identity. The originally submitted scheme fell short of this requirement and failed to meet the needs of local policy and the National Planning Policy Framework in terms of architectural quality and response to local context and was not supported in urban design and conservation terms.

The applicant amended the scheme in recognition of the local level of objection and the objection from the City Design Group (CDG) to the design of the proposed school. The revised scheme has altered the design in the following ways:

- \* Retention of the existing stone boundary wall along Avonvale Road and the creation of a new stone boundary wall along Victoria Avenue.
- \* The reception and main hall would be fully glazed, promoting openness and interaction with the existing street.
- \* The corner treatment close to the main school entrance. The previous 'drum' has been removed and a new feature fin wall would be used as the transition between Victoria Avenue and Avonvale Road. The intention is that the stone used in this wall would be re-used from the existing school buildings.
- \* The materials used on the public elevations have been changed. The plinth and base of the main elevations would be stone, with render used above on the main elevation along Avonvale Road, and a combination of render and cedar cladding, within distinctive bays along Victoria Avenue. The previous use of panelling has completely been omitted from the scheme.
- \* The fenestration and solid to void ratio has been redesigned with a revised composition for all of the public facing elevations.
- \* The solar panels located along the roof of the Victoria Avenue building have been repositioned so that they sit further on the flat roof of this building and are less visible from the street. To counter the loss of the previously overhanging PV panels, the applicant has raised the height of the cedar clad bays that would be slightly higher than the rendered bays to add interest at the parapet level of this building.

The City Design Group has been involved in this proposal since the pre-application enquiry was submitted in May 2013. They acknowledge that it is regrettable that the scheme requires the total demolition of the existing school buildings that are of interest within the local townscape. However, it is also accepted that the existing buildings preclude a remodelling for on-going use and whilst the demolition will clearly remove the local character this site has, they are not opposed to their replacement with the proposed new school.

On the basis that demolition was required and accepted, the recommendation to the applicant during early design discussions at the pre-application stage was for them to consider a design that would provide a strong townscape presence with a robust material nature like the existing buildings, expressed using local materials and articulated using generous openings and a playful approach to the elevation treatment. The resulting building should be read as a confident building within the street scene. Schools should appear welcoming, attractive and have an open presence within the local

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neighbourhood. This can be achieved in many ways, through the use of attractive materials, having views from public realm into the school site, the use of artwork and interesting/novel/playful architectural details. These considerations are especially important at this site, given the proposed loss of the existing characterful buildings.

**Layout:**

In principle, the development of the main building being positioned close to the back of pavement (behind the existing retained boundary wall) along Avonvale Road and Victoria Avenue is acceptable in urban design terms, as this addresses the key public routes and acts to shield the school's playground and teaching spaces from traffic noise. It will also provide a strong sense of enclosure to adjacent streets and to the playground giving a sense of public frontage and private interior.

**Massing:**

Moreover, the massing of the building is considered acceptable, with the larger part of the building facing onto Avonvale Road and the smaller/lower portion of the building facing onto Victoria Avenue, and the approach taken to break-down the massing along Victoria Avenue by using different materials to express distinctive bays is supported. The glazing used along the Avonvale Road elevation provides a sense of openness, with views from the public realm into the main entrance and hall.

**Materials:**

The proposal to re-use natural (pennant) stone recovered from the demolition was a requirement made clear during the pre-application stage. Moreover, the retention of any features was also encouraged. This has been achieved by the retention of the existing boundary walls and the feature fin stone wall, which acts as a focus for the school, helping to turn the corner and offer transition between Avonvale Road and Victoria Avenue.

The rest of the material palette has been refined since the original submission and now provides a more simple and attractive appearance, with both Victoria Avenue and Avonvale Road now appearing more co-ordinated as a building. The materials used along the Victoria Avenue elevation of render and cedar cladding define each of the bays, as does the strong vertical emphasis of the fenestration along this elevation of the building.

**Other matters:**

The objection raised by the Police Crime Prevention Design Advisor that related to the fact that the original proposal would result in two telecommunications cabinets being left within the middle of the existing footways and a bench within the footway have been fully addressed by the revised scheme that retains the existing boundary walls. As a result, the revised scheme now has a small area of defensible space between the back edge of the footway and the school buildings, together with visitor cycle parking at the main entrance but behind the retained wall. The removal of a vehicular access at the eastern end of the site, off Avonvale Road, is also regarded by the Police to be a safety and security improvement.

The Police have urged the developer to apply for Secured by Design certification as this would ensure minimum standards of security. An advice note has been included on this matter.

**Conclusion:**

It is considered that the revised scheme has sufficiently addressed the previous objections raised by the CDG and those of local residents. The revised scheme retains the existing stone boundary walls and will introduce a new stone boundary wall along Victoria Avenue, together with re-used stone from

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the existing buildings in the feature 'fin' wall close to the main entrance. The new design overcomes the fortified, monolithic appearance of the original scheme, with a complete change to the fenestration, materials and design of the external elevations. The amended scheme now reads as a more attractive, active and welcoming building, with simple and consistent detailing in respect of the external material palette. Overall, the design of the proposed school is considered to be acceptable and of sufficient quality to comply with policies BCS21, DM29 and DM26 and the advice within the NPPF.

**D) IS THE DEVELOPMENT ACCEPTABLE ON HIGHWAY GROUNDS?**

The site's previous use as Council offices accommodated 160 staff. The proposed school will provide places for 420 pupils and 30 full-time staff. The applicant provided a Transport Assessment (TA) and an Interim Travel Plan (TP) as part of their submission.

The site is within a densely populated residential area, with good links to local facilities. The site is located in a sustainable location, close to Church Road where a major bus route operates and also where there are a range of shops, services and facilities. Given these factors, this is a good location for a new primary school. In addition, there is a shortage of school places in this area, meaning that the attendees will be likely to be local and within a reasonable walking and cycling distance.

The TA has calculated the number of trips associated with the former office use, and compared them with the likely trips associated with the proposed school use. The trip rates are similar and it is unlikely that the school would have a detrimental effect on trip rates. There would be a likely impact at peak hours, with the arrival of around 120 cars in the morning and afternoon peaks. Parking surveys have shown that there is adequate space available on-street to accommodate this, even with the proposed waiting restrictions proposed for the area. It will, however, be necessary to limit these car trips, and a Travel Plan is proposed to keep the numbers of vehicles to a minimum.

**Highway safety & parking considerations**

Objections have been received on the basis of the local roads being a danger to future pupils accessing this site. As part of this proposal there are a number of highway measures proposed to improve pedestrian facilities within the immediate vicinity of this site (refer to Supporting Document 4).

It is proposed to replace the existing refuge island on Avonvale Road with a raised zebra crossing. The raised crossing would highlight the crossing point to all road users and act as a speed deterrent. Moreover, the small raised table at the entrance to Victoria Avenue located close to the proposed visitor's entrance would be replaced with a new surface treatment over an extended area. As part of the surface treatment, cobbles are to be set into the road surface and a different coloured surfacing will be applied to the area. This feature will slow vehicles upon entering Victoria Avenue on approach to the school entrance and highlight the change in environment.

A footway build-out, outside of the pupil entrance on Victoria Avenue and another build-out on the opposite (south) side of Victoria Avenue would provide signify an important crossing point, providing a safe zone for pupils to cross Victoria Avenue and assist in reducing speeds of cars along this road. A flat top speed hump and pavement build out would be constructed close to the junction of Leonard Road and Victoria Avenue. Finally, footway build outs would be provided at the corner of Roseberry Road, with corresponding tactile paving and crossing points to prevent dangerous parking and aid pedestrian safety.

A disabled parking bay would be provided on Victoria Avenue, between the pupil entrance and the raised junction onto Avonvale Road, and a coach drop-off bay would be provided on Avonvale Road, to the north of the Victoria Avenue/Avonvale Road junction outside the main school building.

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School Keep Clear markings are proposed at the main school accesses to deter unsafe parking at key areas of pupil concentration. As a temporary measure, advisory School Keep Clear markings will be in place along Leonard Road for a temporary period (phase 1) until the whole school has been constructed (phase 2).

Objections have been received on the basis that parking congestion will occur as a result of this development. This is likely to be the case at peak times, as is the case with most schools if parents drive to the school, but this situation will subside outside of the peak hours.

The proposed scheme does not include any on-site car parking. This is a deliberate position to remove staff cars from the school grounds, to enable the space to be used safely and unrestricted by pupils and staff. Our Highway Team consider that even with no on-site staff car parking, it will result in fewer staff parking in the local streets than the previous office use, resulting in an improvement on the existing situation.

A waiting restriction scheme is currently being processed separately to provide dropped kerb and junction protection, which will reduce inappropriate parking.

Unlike the previous office use, no car parking is proposed on-site, as there is inadequate space internally to provide this. This is to encourage the use of sustainable modes of transport, but also in recognition of this being a small site in a sustainable location. There are no objections to this, subject to the staff being included within the Travel Plan to allow them to make more sustainable travel choices. There is adequate space to park on-street should this be required, and the impact is considered to be less than the current office car parking, much of which spills onto the streets nearby at present. 30 employees will have less impact than the 160 office staff that were previously employed at this site.

#### Cycle parking

Cycle and scooter parking bays are proposed within the south west corner of the school in a secure shelter. This facility would provide space for 42 bicycles (21 Sheffield type stands) and 30 scooters. Six, undercover and secure cycle parking would be provided for staff within the service area to the north-east corner of the site. A further three Sheffield cycle stands (six cycle parking spaces) would be provided outside the visitor's reception area, behind the retained boundary wall. The total number of cycle parking spaces would accord with the standards set out in the Site Allocation and Development Management Development Plan Document, which has now been approved by the Inspector. A condition to secure all of the cycle and scooter parking is delivered prior to occupation is recommended.

#### Servicing

Servicing would take place from a servicing area located at the northeast corner of the site, with pedestrian access only from Avonvale Road. There are no objections to this on highway safety grounds.

#### Travel Planning

The Interim Travel Plan will form the basis of a full Travel Plan once the proposed school is opened. The Interim Travel Plan states that the full Travel Plan will then follow the format set out by the Bristol School's Travel Plan template. A condition is recommended that requires a full detailed Travel Plan to be prepared and submitted for approval within 6 months following the date of the school being opened and becoming operational.

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## Conclusion

The development is considered to be acceptable on highway grounds, subject to the necessary funds being secured to deliver the highway safety measures and conditions to deliver cycle parking and travel plan requirements.

## E) IS THE DEVELOPMENT ACCEPTABLE ON AMENITY GROUNDS?

There have been no objections received from local residents on amenity grounds relating to privacy, shadowing, overbearing, or loss of light, but these issues have been considered in detail.

The existing site contains marked car parking spaces and this parking was heavily used when the site was last in use as offices. The removal of the on-site parking will assist in improving amenity for the adjoining residents in respect of noise from vehicles, car doors being shut, etc.

The proposed development would result in greater separation distances between the properties along Leonard Road and the existing buildings on-site. The proposed school buildings would, however, be much taller than the existing school buildings, being a combination of 2 and 3 storeys in height.

## Impact upon properties along Leonard Road

The north (inner) elevation of the two storey (9.4m from ground to top of roof) element of the building situated along Victoria Avenue would have windows that look directly north towards the rear of properties along Leonard Road. There would be a window to window separation distance of between 31.5m and 29m between the proposed school and the existing dwellings (between 23 and 26.5m between the proposed school windows and the northern boundary). These distances are considered to be adequate to protect the amenity of the residents of these properties. This distance is also adequate to ensure that no overbearing or loss of light would occur between the proposed school and dwellings.

The north west (inner) elevation of the proposed school (along Avonvale Road) would be 3 storeys high (13m from ground to top of roof) for most of its elevation, with a lower, 1 ½ storey high, section closest to 32 Avonvale Road. The windows on the top (second) floor of this part of the building would serve a staff room, and two year 6 classrooms. The northern most window (i.e. closest to the boundary of Leonard Road) would be 11.4m away from the nearest residential property. This separation distance is considered to be sufficient to protect the amenity of residents of Leonard Road from unacceptable overlooking.

A combination of the distance of the North West elevation from the properties along Leonard Road and the orientation of this wing of the school building means that there are no objections to this part of the building on overbearing or loss of light grounds.

The submitted solar study provides a shadow analysis of the existing and proposed buildings during dates in March, June and December. The only time that there would be any materially greater impact from shadowing would be during March at 9am, when the proposed building along Avonvale Road would result in a minor increase in the shadow cast across the rear gardens of 5 properties along Leonard Road (nos. 3-11 odds). From 9am onwards, this shadow would reduce, and by 12:00 noon, there would be no shadow cast across these rear gardens from the proposed buildings, only from the existing boundary wall. Given this very limited degree of shadowing, it is considered that the proposed school would not amount to unacceptable harm to residential amenity and is acceptable on this basis.

The proposed new buildings would not pose any amenity impact upon the properties along the western part of Leonard Road, given their separation distance.

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## Impact upon properties along Victoria Avenue

The separation distance (across the road) between the windows on the south elevation of the Victoria Avenue wing of the building to the flats on the southern side of Victoria Avenue would be 13.5m. This is considered to be an adequate separation distance that would protect both parties from unacceptable overlooking. It should also be noted that the windows along the ground floor of this elevation of the school would serve breakout areas, toilets, stock rooms, staff offices, a group room and windows serving a stair core and these are rooms that accommodate activities that would naturally not give rise to a perception of overlooking from adjacent occupiers. Moreover, the existing school building already occupies the same position and thus the window to window relationship is largely unchanged, albeit the proposed building is two storeys with windows on both floors, as opposed to the existing single storey building.

The cycle and scooter parking area situated inside the site at the western end of Victoria Avenue would be largely screened from Victoria Avenue and would not create any amenity issues for nearby residents.

## Impact upon properties along Avonvale Road

The east elevation of the proposed school is the highest part of the school and has the greatest visual impact. From ground to the top (leading edge) of the roof this part of the building would be 13m in height (top storey being set back from building edge). However, this part of the site can accommodate this scale, being located adjacent to the principle road that bounds this site. The closest properties to this elevation are situated within Terrell Gardens, with their rear gardens backing onto Avonvale Road. The distance between the proposed school elevation and the rear boundary treatments would range between 13m and 30m. Given this separation distance and the existence of the main road between these buildings, this wing of the building would not give rise to any unacceptable amenity impact, such as a loss of light, overbearing or invasion of privacy.

## Sprinkler tank, refuse and service yard

The sprinkler tank, refuse storage and service yard, all located at the northern end of the site, closer to Avonvale Road would be largely (not totally) screened from view from the properties along Leonard Road and from the adjoining property of 32 Avonvale Road and would not create any unacceptable amenity impact. There would inevitably be some noise impact from the disposal of waste and recycling, together with service deliveries in this part of the site, but this is unlikely to cause harm and would be much less than the authorised on-site car parking and servicing that previously took place for the offices. The revision to the scheme to remove a vehicular access at this point is an improvement and helps to further control unacceptable noise and disturbance arising in this part of the site.

## Playground

Clearly, noise will be created as a result of children playing in the playground during times of outdoor play. However, this site was originally planned, built and operated as a school for many years with the playground right up against existing boundaries of the site. Moreover, the removal of the previous on-site car parking and unrestricted hours of usage associated with the previous office use balances this issue. Overall, it is considered that the use of the playground would not cause unacceptable harm by reason of noise and disturbance to the amenity of local residents.

## On-site trees along Leonard Road boundary

An objection to the trees proposed on site was raised in one of the representations received from local residents (refer to consultations section). This was on the grounds of blocking light and potential for

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root damage to the wall and overhanging branches. The Councils arboriculture officer considers that subject to careful specimen choice and the use of tree pits, these concerns can be addressed. In the event of an approval it is proposed to deal with this matter under condition, which will require a scheme of landscaping to be submitted with suitable species used along this boundary to ensure no unacceptable loss of light (shading) occurs, but enables some tree planting to take place to enhance the appearance of the site and the experience for pupils and staff.

**Games court**

The games court (32m x 19m) would be located in the south west corner of the site. It would not be floodlit or fenced and would consist of surface markings on the tarmac to enable informal games to be played. This facility would not give rise to unacceptable amenity impacts and it is a recognised and accepted facility for a school to have. The western boundary to this site would be 4m high (combination of retained wall and fencing) which is adequate to ensure balls are retained in the site, as best they can, be when being used in the marked games court area.

**Construction**

Construction activities associated with the demolition of the existing school buildings and the construction of the new school will inevitably cause some disruption to local residents. However, this is a temporary situation that can be adequately controlled and managed through then imposition of a Construction Management Plan condition. It is considered that the close relationship of the existing dwelling and the busy highway network that surrounds this site justifies the use of such a condition and this has also been recommended by the Councils Pollution Control team.

In the interests of residential amenity and the quality of the environment created for pupils and staff at the school, conditions are recommended (as suggested by the Pollution Control Team) to control noise from plant and equipment, to ensure details of noise insulation measures and the scheme of mechanical ventilation.

In conclusion of this key issue, it is considered that the proposed development would not cause unacceptable harm to residential amenity and, where appropriate, conditions can be imposed to minimise any adverse impacts.

**F) IS THE DEVELOPMENT ACCEPTABLE ON SUSTAINABILITY GROUNDS?**

The applicant submitted a Sustainability Statement and Energy Strategy. The applicant proposes to reduce energy demand through good levels of thermal insulation, passive ventilation, passive solar design and natural ventilation. The applicant has addressed climate change adaptation with regard to both site layout and built form.

Solar PV panels (13.1kwp) will be located on the flat roof of the building proposed along Victoria Avenue, to provide renewable low carbon energy, meeting the 20% carbon reduction beyond the residual energy base line as required under policy BCS14.

The applicant has submitted a detailed strategy outlining good fabric efficiencies and air tightness. The lighting will have automatic daylight dimming controls and presence/absence detectors to avoid lighting unused areas, which in combination with energy efficient bulbs should see an additional saving of 14% from the lighting load.

The inclusion of a Building Management System and the proposal to link it to a learning resource facility to demonstrate how energy in the building is being used is commended. Moreover, a renewable energy display in the school reception would be provided that accords with the Bristol City Council's Sustainable School Specification.

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The proposed Site Waste Management Plan outlines targets to recycle/recover 97% of demolition waste, and the proposal does include the use of recovered stone from the existing building within the new design (refer to Key Issues B and C) . The retention of the existing boundary walls along Avonvale Road and Leonard Road are welcomed and further reduce waste, whilst maintaining the embodied energy within these structures.

The proposal to use a mechanical ventilation system on the south side of the building for those areas with less demand (i.e. non-teaching spaces) to avoid increased energy use for cooling (which includes a heat recovery system) is welcomed.

The applicant proposes to reduce water consumption by 25% through the use of dual flush WC's, spray, aerator and time controlled taps and through the use of flow restrictors.

The applicant has submitted a BREEAM Assessment that indicates this development is targeting a BREEAM 'Very Good' rating and that this is achievable, with the possibility that it could achieve an 'Excellent' rating. Currently BREEAM Very Good is acceptable under policy and whilst there is an aspiration for all developments to achieve BREEAM Excellent from 2016 onwards, this would not be a reason to refuse this application and the stated rating of 'Very Good' is acceptable and would be conditioned.

#### Conclusion

Overall, there is no objection from the Councils Sustainable City team despite the wholesale demolition. The development would comply with Core Strategy policies BCS13, 14 and 15. On this basis, this development is acceptable on sustainability grounds. Conditions are required to secure the solar panel installation and energy efficiency measures, and that BREEAM 'Very Good' is achieved.

#### G) DOES THE DEVELOPMENT PROVIDE ADEQUATE OUTDOOR PLAY FACILITIES?

The agent was asked to provide details of the external areas relating to outdoor play and whether the area proposed met the required level required under current relevant guidance.

The overall site area is 4,109m<sup>2</sup>, with 2,191m<sup>2</sup> of external space available once the proposed school has been built. This breaks down into 1,981m<sup>2</sup> for hard play and games court and a habitat area of 210m<sup>2</sup>. In accordance with Building Bulletin 99 (used for schools - still the latest guidance), the size of this site falls into the category of a 'confined' site. The proposed amount of hard play and games court provision would be 361m<sup>2</sup> above that recommended by BB99 for a confined site, with the habitat area being on target to meet the recommendation for a confined site.

In discussions with the agent, it has been confirmed that the area for the Games Court would be more useful if it is not separately fenced, enabling the space to be used flexibly, either as one space for large events/play area, or as a specific games court when the need arises. Soft play areas, such as grassed surfaces, given the confined size of this site are not considered practical as a long term solution at this site. As such, the provision for soft play on playing fields/pitches would be catered for off-site at a suitable playing/sports field.

In conclusion of this issue, the playground proposed would exceed the necessary standard for hard play and a games court provision for a confined site. The incorporation of a habitat/wildlife area is welcomed and this also meets the current standard. The landscaping of the habitat area would be conditioned, but other habitat measures are best left to the school, so that children can take an active part in developing this space.



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Moreover, the fact that the playground would be sheltered from road traffic noise creates a better outdoor play experience than the existing playground arrangement. Overall, adequate outdoor play space is provided which will be better than the previous provision at this site.

**H) DOES THE DEVELOPMENT PROVIDE THE NECESSARY S106 CONTRIBUTIONS?**

The only necessary obligations relate to highway matters.

There are out-dated traffic management measures in place near this site that are not effective enough to adequately cater for the proposed number of pupil movements at peak times.

The improvements to safety, such as the provision of a raised zebra crossing where there is currently a traffic island, proposed treatment to the junction of Victoria Avenue to highlight the presence of pedestrians, the provision of 'School Keep Clear' markings at the main school accesses to deter unsafe parking at areas of pupil concentration, and the marked disabled and coach parking bays have been secured under a Memorandum of Understanding by the Highway Authority on behalf of the Local Education Partnership (LEP).

**I) HAVE MATTERS RELATING TO AIR QUALITY, ARCHAEOLOGY, ECOLOGY, FLOOD RISK AND GROUND CONTAMINATION BEEN ADEQUATELY ADDRESSED?****Air Quality:**

An Air Quality Assessment (AQA) report was submitted with this application. The assessment considered dust effects during the construction phase, together with the suitability of the site for its intended use in the context of air quality.

The site falls within a designated Air Quality Management Area (AQMA) due to high levels of nitrogen dioxide (NO<sub>2</sub>) and particulate matter (PM<sub>10</sub>) attributed to road traffic emissions.

Impacts during the construction of the proposed development, such as dust and plant/vehicle emissions, would only be of a short duration and only relevant during the construction phase. The consultants who prepared the AQA undertook a risk assessment of construction dust impacts, which suggests that without mitigation and controls the significance of the dust effect is likely to be 'slight adverse' to 'moderate adverse'. Implementation of the highly recommended mitigation measures for developments having an overall 'medium' Dust Impact Risk (as set out in the IAQM Dust and Air Emissions Mitigation Measures document), should reduce the significance of the dust effects to 'negligible' to 'slight adverse'.

The AQA states that pollutant concentrations are predicted to be below the relevant objectives at the site and that this site is considered to be suitable, in air quality terms, for its intended use. This conclusion is accepted, and the proposed development does not conflict in air quality terms with measures set out in the Councils 'Air Quality Action Plan'. On this basis, there are no objections to this development on the basis of air quality. Matters relating to dust and emissions from plant and equipment during the construction phase can be adequately controlled under both the Construction Management Plan and the relevant environmental health legislation.

**Archaeology:**

The applicant submitted an archaeological desk-based assessment in support of this application.

The site lies in an area of possible archaeological significance. A number of late Roman burials were located to the south of the site, initially in the late 19th century and then more recently in 2002 and 2007. These were all found on the south side of Victoria Avenue. The presence of these burials, especially those found in the 19th century, which were found in rows, strongly suggests the presence

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of a Roman cemetery of unknown dimensions. There were further indications of Roman settlement from a limited evaluation to the east of this site. This work, especially the occurrence of burials close to the site, suggests that similar finds might be made at this site. On the basis of this information, the Council's archaeologist has recommended an archaeological evaluation be carried out as early as possible to define the archaeological potential of the site and allow for an informed decision on the most appropriate form of mitigation to be adopted.

The school building itself has been nominated for the Bristol Local List and is clearly a building of some architectural merit. However, on the basis that the case has been made for demolition and the building cannot be saved, it is important that it is fully recorded both internally and externally to provide a permanent record, which can be secured by conditions.

**Ecology:**

The existing site does not contain any wildlife and the demolition of the existing buildings does not pose any issues for biodiversity. The proposed scheme would incorporate a wildlife/habitat area and landscaping behind the retained and new stone boundary walls, together with new tree planting within the school playground. These new features would enhance the biodiversity of this site, and the development is considered to be acceptable in respect of this issue.

**Flood Risk & Drainage:**

The application was supported by a Flood Risk Assessment and a Drainage Strategy Report.

The site has an area of 4,100 m<sup>2</sup> and being occupied by existing buildings, car park and play areas, approximately 100% of the site is impermeable. Records show that the site is located within Flood Zone 1. This zone is land assessed as having less than a 1 in 1000 annual probability of river or sea flooding in any year. These areas would have no constraints on development other than the need to ensure that the development does not increase run-off from the site to greater than that from the site in its undeveloped or presently developed state.

**Ground water:** A geotechnical investigation has been carried out. The Drainage Strategy report indicates that ground water was encountered at depths of 3.7 - 5.7m below the existing site surface, and on this basis, groundwater does not present a significant risk to the development.

**Overland Flow:** The existing site generally falls from north to south and slightly from east to west. There are no known overland flood issues.

**Infrastructure Failure / Sewer Flooding:** The Drainage Strategy report indicates that micro-drainage modelling and simulation of the existing drainage network within the school have been undertaken. The analysis showed that for a 1 in 100yr + 20% CC storm category the network will experience a minor flood.

The new proposed teaching block would discharge via a new foul water drainage network which will connect onto the existing private network, and all new foul drainage will be designed in accordance with Building Regulations Part H, BS EN 752 and Sewers for Adoption.

**Surface Water Strategy:** The Drainage Strategy indicates that it is proposed to have a new surface water drainage network that would collect surface water run-off from the proposed teaching block and playground area.

The proposed teaching block would be drained with rainwater down pipes that connect directly into a new surface water network which will be attenuated. The playground area is likely to be connected with a porous surfacing attenuated with flow control devices restricting flow prior to discharging to the

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new surface water network. All new surface water drainage network will be modelled to not flood in a storm event of 1 in 100yr +20%CC and to ensure that the modelled flooding on the existing network is not increased and where possible improved.

In accordance with Core Strategy policy BCS16, it is proposed that the sites location in Flood Zone 1, together with a 30% reduction in run-off rates that would be achieved when compared to existing flow rates, makes the development acceptable on flood risk and drainage grounds.

In accordance with advice from the Councils Flood Risk/Drainage team, a condition is required that secures further details of the drainage strategy for approval.

**Ground Contamination:**

The site lies within a 'high-risk' coal mining area, and the applicant provided a Coal Mining Risk Assessment as part of their application. This indicated that the site is in a likely zone of influence from workings in 5 seams of coal at 480m to 730m depth, of which were last actively worked in the 1920's. Rotary probes were sunk into the ground on site to a depth of 30m and no coal seams or cavities were encountered, and shallow mine workings would have been at a depth of 30m. The report concludes that the risk to the development from historically worked seams, opencast mining and mine gas is low. There is evidence of a shaft within 20 metres of this site, located to the east on the opposite side of Avonvale Road, but this is not considered to pose any threat to new development taking place on this site. The foundations of the new development would be at a depth of 2m and would, therefore, lie outside of the zone of influence of any future collapse of shaft wall.

The Coal Authority have confirmed that the proposal is acceptable and not at risk from any historically worked seams near to this site.

**J) CONCLUSION**

The principle of the loss of the office use at this site, to enable this site to be used, once again, as a primary school, is acceptable.

There is an acute need for the establishment of a new primary school in this part of Bristol; this school will meet that need and allow children from this local area to attend this school. The demolition of the existing buildings is regrettable, but they are not protected by virtue of being either listed or within a conservation area. Your officers did hold a series of discussions during the course of the pre-application enquiry to try and find a solution that could retain and convert either all or some of the existing buildings, but this was concluded not be feasible in this case. The revised design for the school has addressed the previous urban design objections. The final proposed design is much more open and inviting, signifying its function. The design incorporates and re-uses stone from the existing school buildings, together with the retention of the existing stone boundary wall for the majority of the boundary. The amount of outdoor (hard) play space exceeds the requirement for a confined school site, and provision of a habitat area is welcomed. The whole of the playground would be largely sheltered from the traffic movements and noise associated with Avonvale Road and this will provide a pleasant environment for the future attendees at this site.

The proposal is acceptable on highway grounds. The site is located in a sustainable location, close to Church Road where a major bus route operates and also where there are a range of shops, services and facilities. The development will deliver required upgrades and improvements in pedestrian safety and movement that mitigate its future impact and ensure the immediate environment around this school site provides a safe environment for all highway users.

The impact from this development on the amenity of neighbouring residents has been carefully assessed and has been found to be acceptable, with no unacceptable harm to local amenity.

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The development complies with sustainability policies, and would achieve BREEAM Very Good and the provision of solar PV panels (13.1kwp) would provide renewable low carbon energy, meeting the 20% carbon reduction as required under policy BCS14.

The environment created for the future school children would be acceptable, making the best use of the available space.

The development is acceptable on air quality grounds and matters relating to archaeology, ecology and flood risk have been adequately addressed and, where appropriate, conditions are recommended.

For all of the reasons specified above, the application is recommended for approval, subject to conditions and the completed legal agreement.

**COMMUNITY INFRASTRUCTURE LEVY**

How much Community Infrastructure Levy (CIL) will the development be required to pay?

This development is liable for CIL, however the CIL rate for this type of development, as set out in the CIL Charging Schedule, is £nil and therefore no CIL is payable.

**RECOMMENDED GRANT subject to condition(s)****Time limit for commencement of development**

1. Full Planning Permission

The development hereby permitted shall begin before the expiration of three years from the date of this permission.

Reason: As required by Section 91 of the Town and Country Planning Act 1990, as amended by Section 51 of the Planning and Compulsory Act 2004.

**Pre commencement condition(s)**

2. No development shall take place (including demolition) until a phasing plan setting out the build programme for the development has been submitted to and approved in writing by the Local Planning Authority prior to the commencement of the development (including demolition) hereby approved. The development shall be implemented in accordance with the agreed phasing plan/programme, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure that each element of the development would be completed in a satisfactory manner.

3. No development (excluding demolition) shall take place until a detailed scheme of noise insulation measures in accordance with the recommendations made in the Environmental Noise Survey Report: 114686-AC-1v1 (dated 3 April 2013) has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include details of sound insulation performance specifications for windows, ventilation systems and noise attenuation.

The scheme of noise insulation measures shall be prepared by a suitably qualified acoustic consultant/engineer and shall take into account the recommendations detailed in the Noise

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Assessment submitted with the application and the provisions of BS 8233: 1999 "Sound Insulation and Noise Insulation for Buildings - Code of Practice", and specifically Building Bulletin 93 'Acoustic Design of School' (BB93).

The approved details shall be implemented in full prior to the commencement of the use permitted and be permanently maintained.

Reason: In the interests of ensuring an acceptable environment within the school.

4. No development (excluding demolition) shall take place until there has been submitted to and approved in writing by the Local Planning Authority a scheme of mechanical ventilation for all necessary rooms. The approved scheme of mechanical ventilation shall be installed and be operational prior to the first occupation of the school hereby approved and retained at all times thereafter.

Reason: To ensure an acceptable environment within the school.

5. No development (excluding demolition) shall take place until there has been submitted to and approved in writing by the Local Planning Authority a scheme of hard and soft landscaping (to include details of the proposed tree planting and habitat area within the playground). The approved scheme shall be implemented so that planting can be carried out during the first planting season following the occupation of the proposed building, or the completion of the development whichever is the sooner. All planted materials shall be maintained for five years and any trees or plants removed, dying, being damaged or becoming diseased within that period shall be replaced in the next planting season with others of similar size and species to those originally required to be planted unless the council gives written consent to any variation.

Reason: To protect and enhance the character of the site and the area and to ensure its appearance is satisfactory.

6. Prior to the relevant works commencing, detailed drawings, including sections, at a (minimum) scale of 1:50; of
  1. The cycle and scooter parking shelter(s)
  2. All canopies
  3. Refuse stores
  4. Sprinkler tank
  5. Soffits and eaves details
  6. External windows and doors and window reveals
  7. Any grills, vents or louvers
  8. Gates onto Avonvale Road and Victoria Avenue
  9. Boundary fencing
  10. Solar panels
  11. School name signage on Avonvale Road elevation.

shall be submitted to and approved in writing by the Local Planning Authority. The detail thereby approved shall be carried out in accordance with that approval.

Reason: In the interests of visual amenity and the character of the area.

7. Prior to the first occupation of the new school, hereby approved, details of the means of ventilation for the extraction and dispersal of cooking smells/fumes, including details of its method of construction, odour control measures, noise levels, its appearance and finish shall be submitted to and approved in writing by the Local Planning Authority. The approved

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scheme shall be installed before the use hereby permitted commences and retained thereafter.

Reason: These details need careful consideration and formal approval and to safeguard the amenity of adjoining properties and to protect the general environment.

8. No development (excluding demolition) shall take place until an assessment to show that the rating level of any plant & equipment, as part of this development, will be at least 5 dB below the background level has been submitted to and approved in writing by the Local Planning Authority.

The assessment must be carried out by a suitably qualified acoustic consultant/engineer and be in accordance with BS4142: 1997- "Method of rating industrial noise affecting mixed residential and industrial areas".

Reason: In order to safeguard the amenities of adjoining residential occupier.

9. To secure the conduct of a watching brief during development groundworks

The applicant/developer shall ensure that all groundworks, including geotechnical works, are monitored and recorded by an archaeologist or an archaeological organisation to be approved by the council and in accordance with the Written Scheme of Investigation approved under condition 11.

Reason: To record remains of archaeological interest before destruction.

10. The development shall take place in accordance with the approved Construction Management Statement, dated February 2014.

Reason: To ensure the effects of the construction process minimise any potential amenity and highway safety impact to an acceptable degree.

11. Sample panels of the external stone work, brick work, render and timber cladding demonstrating the colour, texture, face bond and pointing/jointing are to be erected on site and approved in writing by the Local Planning Authority before the relevant parts of the work are commenced. The development shall be completed in accordance with the approved details before the building is occupied.

Reason: In order that the external appearance of the building is satisfactory.

12. No development (including demolition) shall take place until the applicant/developer has secured the implementation of a programme of archaeological work, in accordance with a Written Scheme of Investigation which has been submitted by the developer and approved in writing by the Local Planning Authority.

The scheme of investigation shall include an assessment of significance and research questions; and:

- \* The programme and methodology of site investigation and recording
- \* The programme for post investigation assessment
- \* Provision to be made for analysis of the site investigation and recording
- \* Provision to be made for publication and dissemination of the analysis and records of the site investigation

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\* Provision to be made for archive deposition of the analysis and records of the site investigation

\* Nomination of a competent person or persons/organisation to undertake the works set out within the Written Scheme of Investigation.

Reason: To ensure that archaeological remains and features are recorded prior to their destruction.

13. To secure the recording of the fabric of buildings of historic or architectural importance

No redevelopment or refurbishment of the existing school buildings shall take place until the applicant/developer has recorded those parts of the building which are likely to be disturbed or concealed in the course of redevelopment or refurbishment. The recording to be carried out by an archaeologist or archaeological organisation approved by the Local Planning Authority.

Reason: To ensure that features of archaeological or architectural importance within a building are recorded before their destruction or concealment.

14. Prior to the installation of any new external lighting being installed and becoming operational, details of the lighting, including Lux levels and light spill, shall be submitted to and approved in writing by the Local Planning Authority. Thereafter, only the approved lighting shall be installed and operational.

Reason: In the interest of residential amenity.

15. Samples or manufactures details of the roof coverings and window and door frames, shall be submitted to and be approved in writing by the Local Planning Authority before the relevant parts of the work are commenced. The development shall be completed in accordance with the approved samples before the building is occupied.

Reason: In order that the external appearance of the building is satisfactory.

16. No development (excluding demolition) shall take place until a detailed drainage scheme, based on the submitted Flood Risk Assessment and Drainage Strategy Report (report no. AVP-STL-D-RPT-0001 RO2, dated 21 March 2014) has been submitted to and approved, in writing by the Local Planning Authority. The development shall take place in accordance with the approved details.

Reason: In the interests of ensuring an acceptable drainage strategy.

**Pre occupation condition(s)**

17. All of the cycle and scooter parking hereby approved shall be installed and made available for use prior to the first occupation of the new building hereby approved. The cycle and scooter parking shall thereafter be retained for the parking of cycles and scooters.

Reason: To ensure that the cycle parking/scooter parking is delivered in the interests of sustainable transport.

18. No building or use hereby permitted shall be occupied or the use commenced until the refuse store, and area/facilities allocated for storing of recyclable materials, as shown on the approved plans have been completed in accordance with the approved plans and the further details to be agreed under condition. Thereafter, all refuse and recyclable materials associated with the development shall either be stored within this dedicated store/area, as shown on the

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approved plans, or internally within the building(s) that form part of the application site. No refuse or recycling material shall be stored or placed for collection on the public highway or pavement and shall be collected from within the site.

Reason: To safeguard the amenity of the occupiers of adjoining premises, protect the general environment, and prevent obstruction to pedestrian movement, and to ensure that there are adequate facilities for the storage and recycling of recoverable materials.

19. Within six months following occupation of the proposed building hereby approved a Travel Plan comprising immediate, continuing and long-term measures to promote and encourage alternatives to single-occupancy car use for staff and pupils shall be submitted to and approved in writing by the Local Planning Authority. The approved Travel Plan shall then be implemented, monitored and reviewed in accordance with the agreed Travel Plan Targets to the satisfaction of the council.

Reason: In order to deliver sustainable transport objectives including a reduction in single occupancy car journeys and the increased use of public transport, walking and cycling.

20. The solar panels hereby approved shall be installed prior to the first occupation of the building hereby approved and retained and maintained for energy generation at all times thereafter, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure that the solar panels are installed and operational in the interests of sustainable development.

21. The development hereby approved shall be undertaken in accordance with the 'Site Waste Management Plan', dated February 2014.

Reason: To minimising construction site waste in the interests of sustainable development.

22. The development hereby approved shall be undertaken in accordance with the Bristol Core Strategy Compliance Report (report no. AVP CH2 RPT 0005 R02, dated February 2014).

Reason: In the interests of sustainable development and energy efficiency.

23. No development shall take place until evidence that the development is registered with a BREEAM certification body and a pre-assessment report (or design stage certificate with interim rating if available) has been submitted indicating that the development can achieve the stipulated final BREEAM level. The proposed building shall not be occupied until a final Certificate has been issued certifying that BREEAM (or any such equivalent national measure of sustainable building which replaces that scheme) rating 'Very Good' has been achieved for this development unless the Local Planning Authority agrees in writing to an extension of the period by which a Certificate is issued.

Reason: To ensure that the development achieves BREEAM rating level Very Good (or any such equivalent national measure of sustainability for building design which replaces that scheme) and assessment and certification shall be carried out by a licensed BREEAM assessor and to ensure that the development contributes to mitigating and adapting to climate change and to meeting targets to reduce carbon dioxide emissions.

24. To ensure completion of a programme of archaeological works

No building shall be occupied until the site investigation and post investigation assessment has been completed in accordance with the programme set out in the Written Scheme of



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Investigation approved under condition 11 and the provision made for analysis, publication and dissemination of results and archive deposition has been secured.

Reason: To ensure that archaeological remains and features are recorded and published prior to their destruction.

25. Prior to the first occupation of the proposed new school buildings hereby approved, unless an alternative timetable is first submitted to and agreed in writing by the Local Planning Authority, the highway works shown on drawing no. RS13025-1 Revision B: 'Proposed Highway Improvement Works' shall be completed to the satisfaction, in writing, of the Local Planning Authority.

Reason: To ensure that the proposal is acceptable on highway safety grounds.

26. The feature 'fin' stone wall near the main school entrance shall be constructed using natural pennant stone taken (re-used) from the existing school buildings, and it shall be completed prior to the first occupation of the school hereby approved, to the satisfaction, in writing of the Local Planning Authority.

Reason: To ensure that some of the existing natural stone from the existing buildings is re-used in the new school building, in interests of visual appearance and the character and appearance of the surrounding area.

**Post occupation management**

27. The existing stone boundary wall along Avonvale Road, return section along Victoria Avenue, together with the wall along Leonard Road as shown on drawing AVP-BMD-A-DWG-3011-PL2 shall be retained in full and shall not be altered in height or form, other than the removal of the pedestrian entrance archway, unless otherwise first agreed in writing by the Local Planning Authority.

Reason: In the interests of preserving the character of this site and its contribution to the local area.

**List of approved plans**

28. List of approved plans and drawings

The development shall conform in all aspects with the plans and details shown in the application as listed below, unless variations are agreed by the Local Planning Authority in order to discharge other conditions attached to this decision.

1501-PL1 Site Location Plan - Application Site - Red Line, received 14 February 2014  
 1504-PL3 Landscape Strategy Plan - Phase 2, received 8 May 2014  
 1505-PL2 Landscape Strategy Plan - Phase 1, received 8 May 2014  
 1101 PL1 Reception 2014 - Ground Floor Plan - Proposed, received 14 February 2014  
 1514-PL2 Proposed Roof Plan, received 8 May 2014  
 1502-PL1 Existing Survey Plan, received 14 February 2014  
 1511-PL2 Proposed Ground Floor Plan, received 8 May 2014  
 1513-PL2 Proposed Second Floor Plan, received 8 May 2014  
 3014-PL2 Proposed Elevations - Materials (1 of 2), received 8 May 2014  
 3015-PL2 Proposed Elevations - Materials (2 of 2), received 8 May 2014  
 3011-PL2 Proposed Site Elevations, received 8 May 2014  
 1512-PL2 Proposed First Floor Plan, received 8 May 2014

**Development Control Committee A – 18 June 2014****Application No. 14/00649/F: Social Services Site (Formerly Avonvale Primary School) Avonvale Road Bristol BS5 9RH**

3013-PL2 Proposed Elevations (2 of 2), received 8 May 2014  
 3001-PL1 Existing Site Elevations, received 14 February 2014  
 3012-PL2 Proposed Elevations (1 of 2), received 8 May 2014  
 Air Quality Report, received 14 February 2014  
 Bristol City Council Core Strategy compliance report, received 14 February 2014  
 Breeam report, received 14 February 2014  
 Brukl Output document, received 14 February 2014  
 Construction Management Statement, received 14 February 2014  
 Ecology report, received 14 February 2014  
 Environmental Noise Survey report, received 14 February 2014  
 Interim Travel Plan, received 14 February 2014  
 Project Environmental Management Plan, received 14 February 2014  
 Site Waste Management Plan, received 14 February 2014  
 Drainage Statement, received 14 February 2014  
 002 PL01 Design and Access Statement - Addendum, received 8 May 2014  
 RS13025-1 Proposed highway improvement works in association with new school, received

Reason: For the avoidance of doubt.

**Advices**

1. Construction site noise: Due to the proximity of existing noise sensitive development and the potential for disturbance arising from contractors' operations, the developers' attention is drawn to Section 60 and 61 of the Control of Pollution Act 1974, to BS 5228: Part 1: 1997 - "Noise and Vibration Control on Construction and Open Sites Code of practice for basic information and procedures for noise and vibration control" and the code of practice adopted by Bristol City Council with regard to "Construction Noise Control". Information in this respect can be obtained from Pollution Control, Brunel House, St. George's Road, Bristol BS1 5UY.
2. Noise - plant & equipment  
  
Anti-vibration mounts should be used to isolate plant from fixed structures and a flexible connector used to connect the flue to the fan if there is a potential to transmit vibration to any noise sensitive property. Any systems will also need regular maintenance so as to reduce mechanical noise.
3. Right of light: The building/extension that you propose may affect a right of light enjoyed by the neighbouring property. This is a private right which can be acquired by prescriptive uses over 20 years; as such it is not affected in any way by the grant of planning permission.
4. BS Standard - tree work: Any works should be completed in accordance with British Standard 3998: Recommendations for tree work, you are advised that the work should be undertaken by a competent and suitably qualified tree contractor.
5. Nesting birds: Anyone who takes, damages or destroys the nest of any wild bird whilst that nest is in use or being built is guilty of an offence under the Wildlife and Countryside Act 1981 and prior to commencing work you should ensure that no nesting birds will be affected.
6. Bats and bat roosts: Anyone who kills, injures or disturbs bats, obstructs access to bat roosts or damages or disturbs bat roosts, even when unoccupied by bats, is guilty of an offence under the Wildlife and Countryside Act 1981, the Countryside and Rights of Way Act 2000 and the Conservation (Natural Habitats, &c.) Regulations Act. Prior to commencing work you should ensure that no bats or bat roosts would be affected. If it is suspected that a bat or bat

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roost is likely to be affected by the proposed works, you should consult English Nature (Taunton office 01823 283211).

7. Wessex Water requirements: It will be necessary to comply with Wessex Water's main drainage requirements and advice and further information can be obtained from <http://www.wessexwater.co.uk>.
8. You are advised to apply for Secured by Design certification as this would ensure minimum standards of security. Further information on Secured by Design initiative can be found at [www.securedbydesign.com](http://www.securedbydesign.com)

**BACKGROUND PAPERS**

Transport Development Management, City Transport	25 March 2014
Urban Design	4 June 2014
Pollution Control	23 May 2014
Sustainable Cities Team (Mark Leach)	9 April 2014
	28 May 2014
Archaeology Team	27 February 2014
Crime Reduction Unit	1 April 2014
Flood Risk Manager	17 March 2014
English Heritage	30 May 2014
The Victorian Society	30 May 2014

Project:  
**Bristol Schools**

- Notes:
1. Do not scale from this drawing.
  2. Site verify all dimensions prior to construction
  3. Report all discrepancies to the Drawing Originator immediately
  4. This drawing is to be read in conjunction with all relevant documents and drawings

PL1	10/02/14	Planning - CB
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Rev. Code	Date	Drawn By - Description
Revisions		

Site key plan

Grid reference (000000,000000)

Skanska Bristol Schools  
240 Park Avenue  
Aztec West  
Almondsbury  
Bristol  
BS32 4SY  
T: +44 (0)1923 776666

Drawing Originator

**Batterham  
Matthews  
Design Ltd**

1 Tollbridge Studios  
Tollbridge Road  
Bath  
BA1 7DE  
T: 01225 851122  
F: 01225 851155

Drawing Status

**Planning**

Project Name

**Redfield Educate Together Primary Academy**

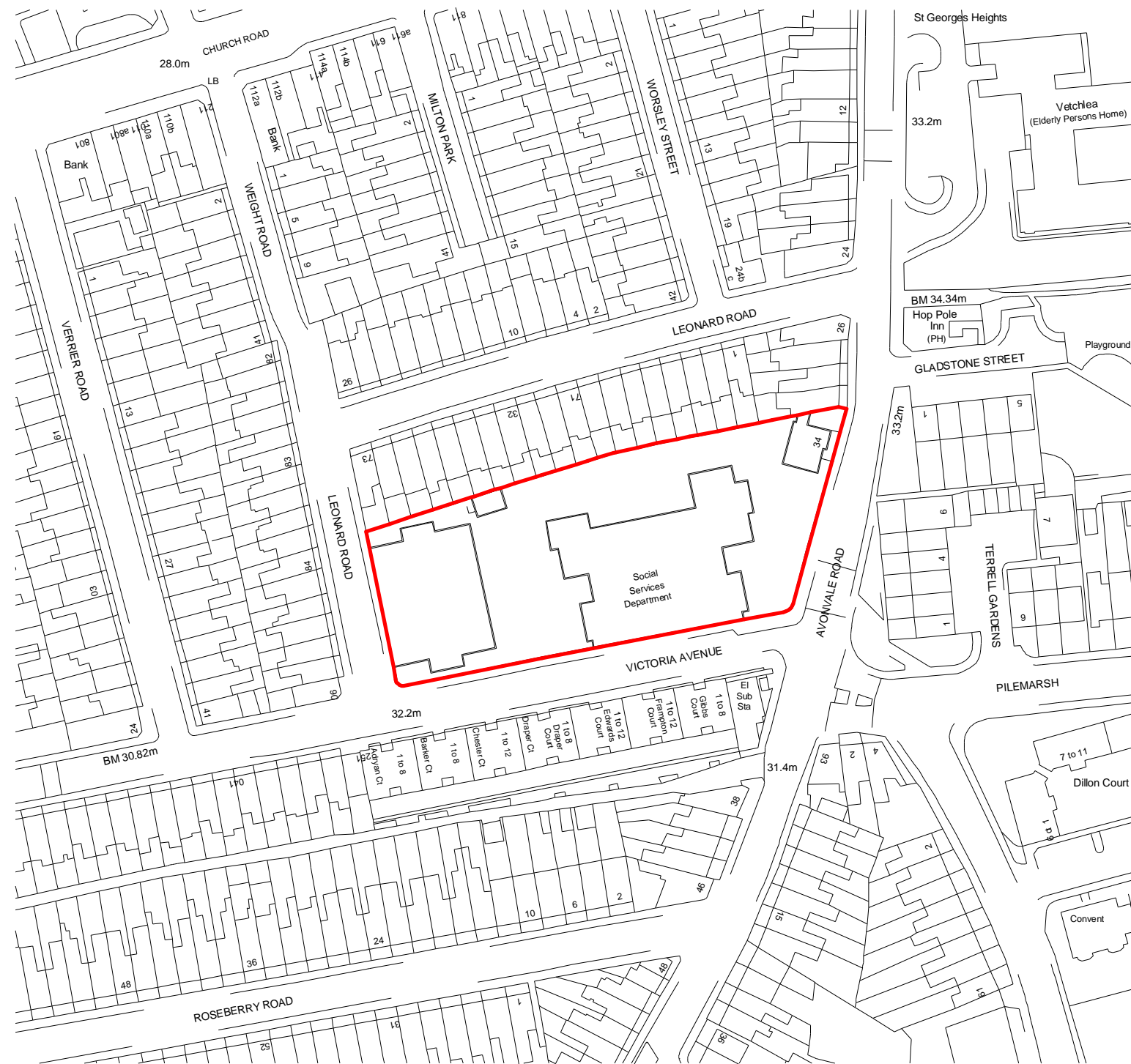
Drawn by <b>CB</b>	Drawn Date <b>01/14/14</b>	Checked by <b>JStL</b>	Checked Date <b>07/02/14</b>
Approved by	Approved Date	Scale <b>1 : 1250</b>	

Title Original drawing sheet is A3

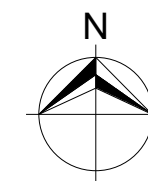
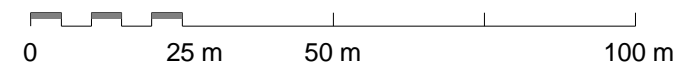
Site Location Plan - Application Site - Red Line

Site code - Originator - Discipline - Document Type - Unique Number - Revision

**AVP-BMD-A-DWG-1501- PL1**



**Site Location Plan**  
1 : 1250





- Notes:  
 1. Do not scale from this drawing  
 2. Site verify all dimensions prior to construction  
 3. Report all discrepancies to the drawing originator immediately  
 4. This drawing is to be read in conjunction with all relevant documents and drawings



PL2	08/05/2014	CB - JSIL - Issued for Planning
PL1	10/02/2014	CB - JSIL - Planning

Rev. Code	Date	Drawn By - Checked By - Description
		Revisions

Site Key Plan:

Grid Reference: (000000,000000)

Skanska Bristol Schools  
 240 Park Avenue  
 Almondsbury  
 Bristol  
 BS32 4SY  
 T: +44 (0)1923 776666

Drawing Originator:  
**Batterham  
 Matthews  
 Design Ltd**

1 Tollridge Studios  
 Tollridge Road  
 Bath  
 BA1 7DE  
 T: 01225 851122  
 F: 01225 851155

Drawing Status:  
**Planning**

Project Name:  
**Redfield Educate Together Primary Academy**

Drawn by:	Drawn date:	Checked by:	Checked date:
CB	03/11/13	JSIL	08/05/14
Approved by:	Approved date:	Scale:	

Title: Original drawing sheet is A1

3D Site View

Site Code-Originator-Discipline-Document Type-Unique Number-	Revision
AVP-BMD-A-DWG-5001-	PL2





1 Planning Elevation - Avonvale Road Elevation  
 1 : 200



2 Planning Elevation - Victoria Avenue Elevation  
 1 : 200



3 Planning Elevation - West Elevation  
 1 : 200

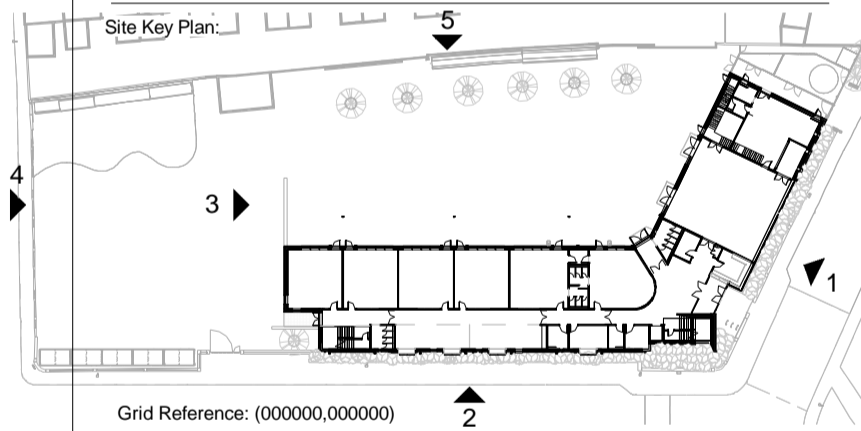


4 Planning Elevation - Leonard Road Elevation  
 1 : 200



5 Planning Elevation - North Elevation - Site Section  
 1 : 200

Rev. Code	Date	Drawn By - Checked By - Description	Revisions
PL2	08/05/2014	CB - JSIL - Issued for Planning	
T01	01/05/2014	CB - JSIL - ISSUED FOR TENDER	
PL1	10/02/2014	CB - JSIL - Planning	



**SKANSKA**  
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 240 Park Avenue  
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 BS32 4SY  
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Drawing Originator:  
**Batterham Matthews Design Ltd**  
 1 Tollridge Studios  
 Tollridge Road  
 Bath  
 BA1 7DE  
 T: 01225 851122  
 F: 01225 851155

Drawing Status:  
 Planning

Project Name:  
 Redfield Educate Together Primary Academy

Drawn by:	Drawn date:	Checked by:	Checked date:
CB	28/11/13	JSIL	08/05/14
Approved by:	Approved date:	Scale:	As indicated

Title:  
 Original drawing sheet is A1

Proposed Site Elevations





1 **Planning Elevation - Avonvale Road Elevation - Hall**  
 1 : 100



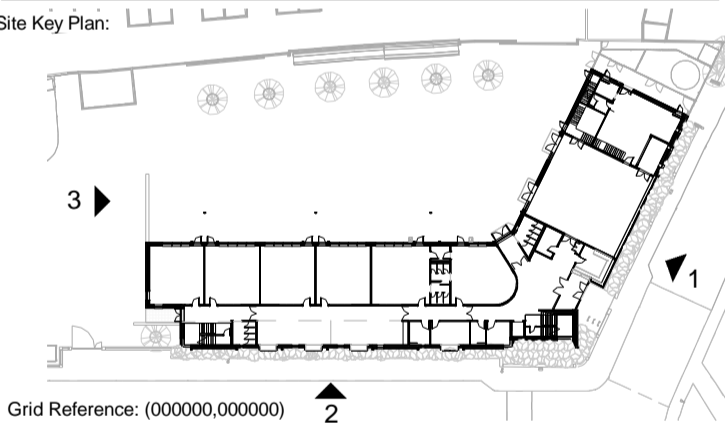
2 **Planning Elevation - Victoria Avenue Elevation - Ancillary and Teaching**  
 1 : 100



3 **Planning Elevation - West Elevation - Classrooms**  
 1 : 100

PL2	08/05/2014	CB - JSIL - Issued for Planning
T01	01/05/2014	CB - JSIL - ISSUED FOR TENDER
P01	03/02/2014	CB - JSIL - Preliminary Issue
PL1	10/02/2014	CB - JSIL - Planning

Rev. Code	Date	Drawn By - Checked By - Description
		Revisions



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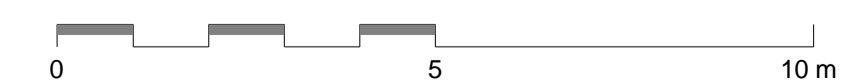
Drawing Status:  
**Planning**

Project Name:  
**Redfield Educate Together Primary Academy**

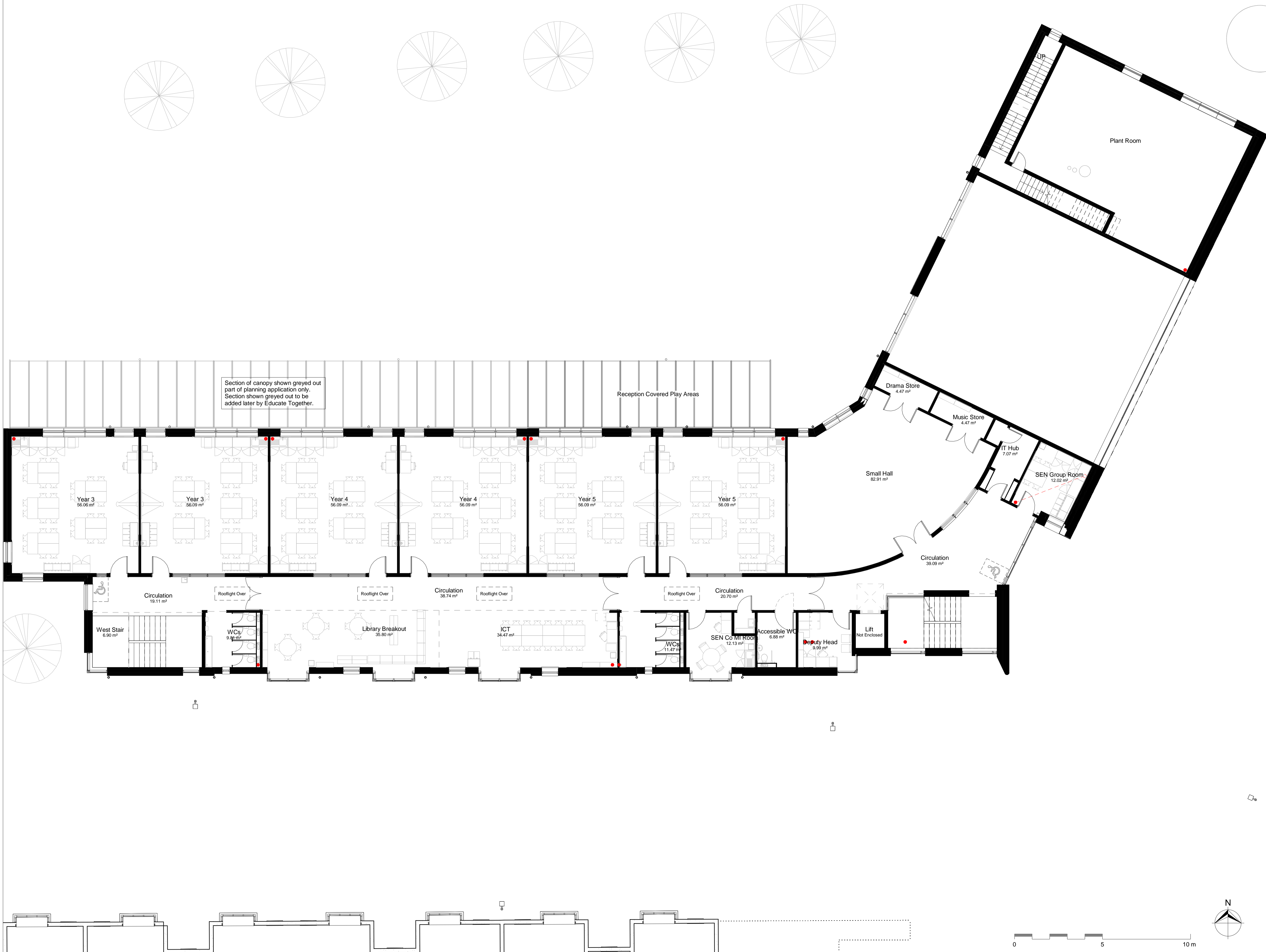
Drawn by: CB	Drawn date: 14/01/14	Checked by: JSIL	Checked date: 08/05/14
Approved by:	Approved date:	Scale: As indicated	

Title: Original drawing sheet is A1

Proposed Elevations - 1 of 2







Section of canopy shown greyed out part of planning application only. Section shown greyed out to be added later by Educate Together.

PL2	08/05/2014	CB - JSIL - Issued for Planning
PL1	10/02/2014	CB - JSIL - Planning
Rev. Code	Date	Drawn By - Checked By - Description Revisions

Site Key Plan:

Grid Reference: (000000,000000)

**SKANSKA**  
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**Batterham Matthews Design Ltd**  
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F: 01225 851155

Drawing Status:  
**Planning**

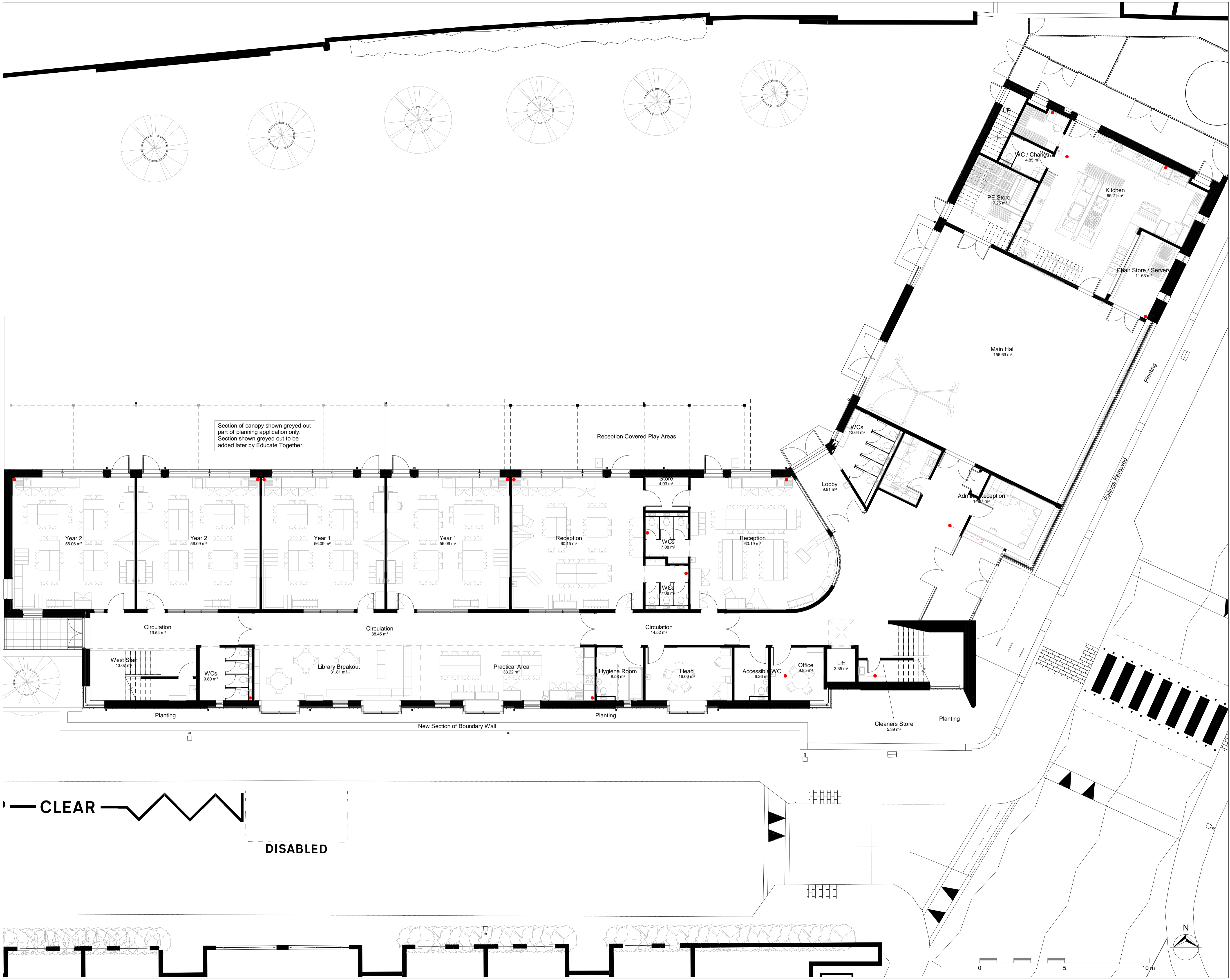
Project Name:  
**Redfield Educate Together Primary Academy**

Drawn by: CB	Drawn date: 14/01/14	Checked by: JSIL	Checked date: 08/05/14
Approved by:	Approved date:	Scale: 1 : 100	

Title:  
**Proposed First Floor Plan**

Site Code-Originator-Discipline-Document Type-Unique Number-	Revision
AVP-BMD-A-DWG-1512-	PL2





Section of canopy shown greyed out part of planning application only. Section shown greyed out to be added later by Educate Together.

PL2	08/05/2014	CB - JSIL - Issued for Planning
PL1	10/02/2014	CB - JSIL - Planning
Rev. Code	Date	Drawn By - Checked By - Description Revisions

Site Key Plan:

Grid Reference: (000000,000000)

**SKANSKA**  
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Drawing Originator:  
**Batterham Matthews Design Ltd**  
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F: 01225 851155

Drawing Status:  
Planning

Project Name:  
Redfield Educate Together Primary Academy

Drawn by: CB	Drawn date: 14/01/14	Checked by: JSIL	Checked date: 08/05/14
Approved by:	Approved date:	Scale: 1 : 100	

Title: Original drawing sheet is A1

Proposed Ground Floor Plan



- Notes:  
 1. Do not scale from this drawing  
 2. Site verify all dimensions prior to construction  
 3. Report all discrepancies to the drawing originator immediately  
 4. This drawing is to be read in conjunction with all relevant documents and drawings



1 **Planning Elevation - North Elevation - Classrooms**  
 1 : 100

Section of canopy part of planning application only. Section to be added later by Educate Together.



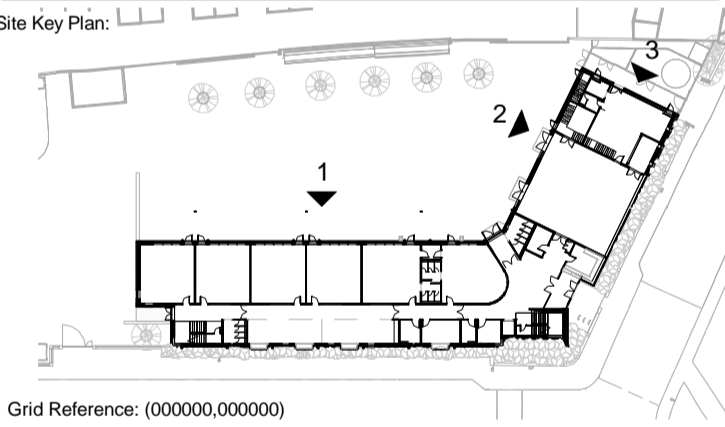
2 **Planning Elevation - North West Elevation**  
 1 : 100



3 **Planning Elevation - North East Elevation**  
 1 : 100

PL2	08/05/2014	CB - JSIL - Issued for Planning
T01	01/05/2014	CB - JSIL - ISSUED FOR TENDER
P01	03/02/2014	CB - JSIL - Preliminary Issue
PL1	10/02/2014	CB - JSIL - Planning

Rev. Code	Date	Drawn By - Checked By - Description	Revisions



**SKANSKA**  
 Skanska Bristol Schools  
 240 Park Avenue  
 Alton West  
 Almondsbury  
 Bristol  
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Drawing Originator:  
**Batterham Matthews Design Ltd**  
 1 Tollridge Studios  
 Tollridge Road  
 Bath  
 BA1 7DE  
 T: 01225 851122  
 F: 01225 851155

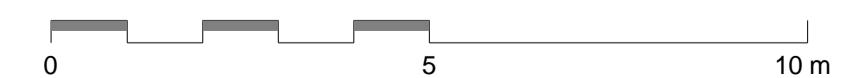
Drawing Status:  
**Planning**

Project Name:  
**Redfield Educate Together Primary Academy**

Drawn by:	Drawn date:	Checked by:	Checked date:
CB	14/01/14	JSIL	08/05/14
Approved by:	Approved date:	Scale:	As indicated

Title: Original drawing sheet is A1

Proposed Elevations - 2 of 2



Site Code-Originator-Discipline-Document Type-Unique Number-	Revision
AVP-BMD-A-DWG-3013-	PL2



landscape key

- existing pavement
- new pavement
- paving slabs
- natural hedge
- new planting
- concrete slab
- low walls
- playground tarmac
- service yard tarmac

PL3	08/05/2014	CB - JSJL - issued for Planning
T01	01/05/2014	CB - JSJL - ISSUED FOR TENDER
PL2	11/03/2014	LB - JSJL - Correction to remove Keep-Clear road markings from Leonard Road
PL1	10/02/2014	CB - JSJL - Planning

Rev. Code	Date	Drawn By - Checked By - Description
		Revisions

Site Key Plan:

Grid Reference: (000000,000000)

Skanska Bristol Schools  
 240 Park Avenue  
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Drawing Originator:

**Batterham  
 Matthews  
 Design Ltd**

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Drawing Status:

Planning

Project Name:

Redfield Educate Together Primary Academy

Drawn by: DS	Drawn date: 30/01/14	Checked by: JSTL	Checked date: 08/05/14
Approved by:	Approved date:	Scale: 1 : 200	

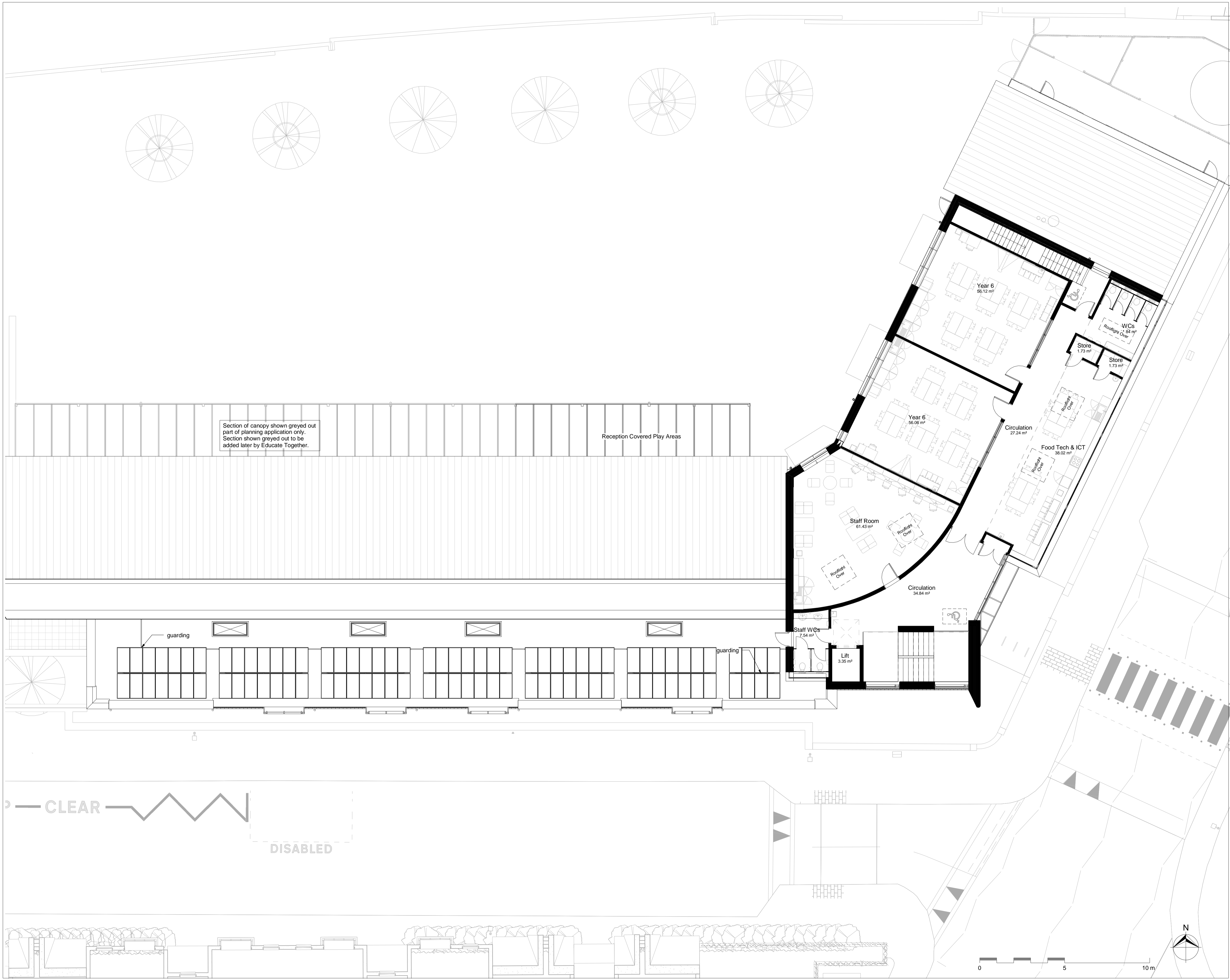
Title: Landscape Strategy Plan - Phase 2

Site Code-Originator-Discipline-Document Type-Unique Number-	Revision
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AVP-BMD-A-DWG-1504- PL3







PL2	08/05/2014	CB - JSIL - Issued for Planning
PL1	10/02/2014	CB - JSIL - Planning
Rev. Code	Date	Drawn By - Checked By - Description Revisions

Site Key Plan:

Grid Reference: (000000,000000)

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Drawing Originator:  
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Matthews  
Design Ltd**

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F: 01225 851155

Drawing Status:  
**Planning**

Project Name:  
**Redfield Educate Together Primary Academy**

Drawn by: <b>CB</b>	Drawn date: <b>14/01/14</b>	Checked by: <b>JSIL</b>	Checked date: <b>08/05/14</b>
Approved by:	Approved date:	Scale: <b>1 : 100</b>	

Title: **Proposed Second Floor Plan** Original drawing sheet is A1

The Local Authority has a statutory duty under the School Standards and Framework Act 1998 to ensure there are sufficient school places, diversity in provision of schools and increased opportunities for parental choice.

Bristol produces a School Organisation Strategy that sets out how the demand for school places will be met. For Primary School provision the Strategy is organised in Neighbourhood Partnership Areas and data is presented on this basis. It is recognised that the actual pattern of school attendance is more complex and many children will cross Neighbourhood boundaries to access school places.

Bristol has seen a rapid growth in demand for primary school places with a 30% rise in the number of pupils entering the Reception Year in the last 5 years. This equates to more than 1,000 extra pupils requiring school places in each year group. The majority of this growth has been accommodated by expansion of existing schools and academies but there is now a need to open new provision in some areas of the city.

The Avonvale Road site is located very close to the boundary between the Ashley, Easton and Lawrence Hill Neighbourhood Partnership Area and the St George East and West Neighbourhood Partnership Area. These areas have seen some of the most significant rises in numbers of children. Both areas are showing a significant shortfall of places if no further action is taken. The projected shortfall across both areas by 2017 is equivalent to 9 forms of entry (270 children each year). The table below shows the number of places available in the Reception year group and the forecast demand for places to 2017.

The majority of schools within the Ashley, East and Lawrence Hill and St George areas have already been expanded or have sites that mean expansion could not be achieved cost effectively. Provision on an additional new site is therefore essential.

The new academy at Avonvale Road will contribute significantly to meeting the shortfall with provision for 2 forms of entry (60 children each year). The site is particularly well-located being located on the boundary between the Neighbourhood Partnership areas.

### Projected Pupil Numbers

#### Ashley , Easton & Lawrence Hill

	2012 (actual)	2013	2014	2015	2016	2017
Admission Number*	613	643	671	671	671	671
Reception Pupil forecast	574	642	717	756	774	798

#### St George East & West

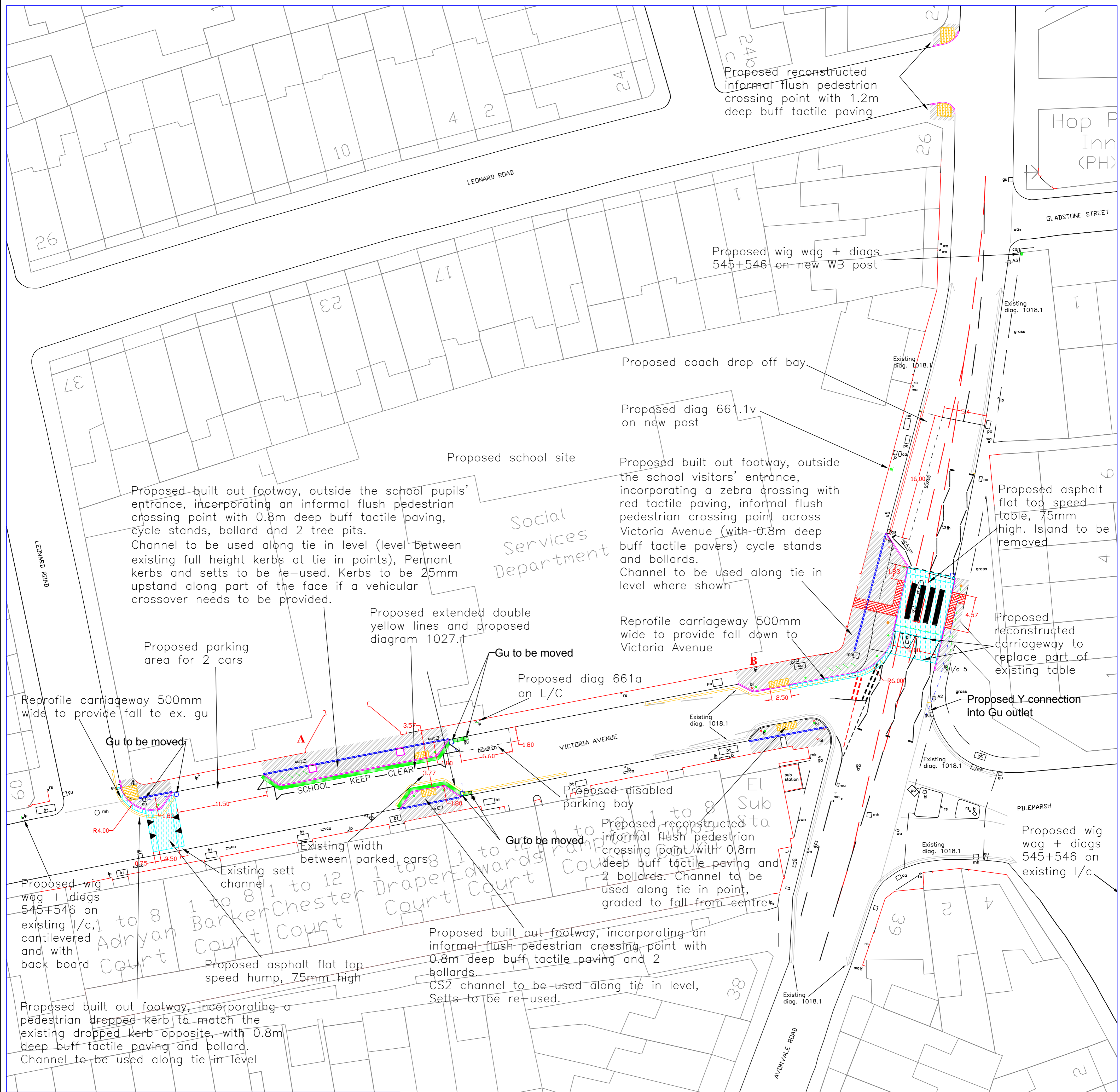
	2012 (actual)	2013	2014	2015	2016	2017
Admission Number*	330	330	330	330	330	330
Reception Pupil forecast	330	339	367	393	431	485

\*not including proposed new academy





















Source: School Organisation Strategy 2013 - 2017

It should also be noted that a bid was made under the Government's 'Targeted Basic Need Programme'. The Avonvale Road scheme was successful in securing £4.354m under this scheme. Under this programme the DfE looked at the shortage of places within the area and allocated funding to the areas with highest need. This funding is specifically for this project and would not be available if the scheme is not delivered.





**Key**

-  Proposed new flexible footway construction
-  Proposed reconstructed carriageway
-  Proposed 125mm upstand PCC HB2 kerb, or other kerb as specified
-  Proposed dropped kerb with buff tactile paving
-  Proposed dropped kerb with red tactile paving
-  Proposed granite setts (previously set aside)
-  Existing road markings to remain
-  Proposed yellow road markings
-  Proposed white road markings
-  Road markings to be eradicated
-  Proposed CS2 PCC channel
-  Proposed duct for electrical feed: 50mm dia. within footway (450mm cover) 150mm + 50mm dia. within carriageway (750mm cover)
-  Proposed 150mm dia. gully connection
-  Proposed PCC gully
-  Proposed tree pit
-  Proposed midubel beacon with floodlamp
-  Proposed bollard (see notes)
-  Bollard to be removed and disposed of
-  Proposed cycle stands
-  Proposed sign and post as detailed

Proposed built out footway incorporating an informal flush pedestrian crossing point with 0.8m deep buff tactile paving. Corresponding crossing point to be constructed opposite

Reprofile carriageway 500mm wide to provide fall down to Avonvale Road

Proposed wig wag + diags 545+546 on existing l/c

**Notes**

Back of footway between points **A** and **B** is proposed to be set back within the school site by approximately 2m, details are to be confirmed.

This will mean that the lamp columns along Victoria Avenue, which are currently at the back of footway, will need to be moved back correspondingly.

Proposed bollards to be Marshalls 'pencil' type.

Rev.	DATE	Description	By

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**Neighbourhoods & City Development**  
 Peter Mann (Service Director)  
 Bristol House  
 St. George's Road  
 Bristol  
 BS1 5UR

**BRISTOL CITY COUNCIL**

**HIGHWAY SERVICES**

Avonvale Road, at its junction with Victoria Avenue  
 Proposed highway improvement works  
 in association with new school  
 General arrangement - draft construction

Dwg **RS13025-1**

Revision **b** Drawn **DS**

Scale 1/250 @ A1 Date February 2014